

# Albany Waterfront Planning Framework Report

2<sup>nd</sup> Edition

Last modified 17/10/11



# Structure Plan Report

## CONTENTS

INTRODUCTION	3
STATUTORY PLANNING	6
STRUCTURE PLAN	
Key Determinants	7
Objectives	8
Design Overview	9
Precincts	13
Uses	14
Parking	15
Built Form	16
Public Spaces	18
Landscape	20
Pedestrian Movement and Access	22
Traffic	23
Services	24
Subdivision	26

Submitted by Hames Sharley on behalf of LandCorp in February 2006 and revised for incorporation into public advertising in July 2011 to include changes involving an increase in area of the hotel, short-stay and mixed-use components of the waterfront development.

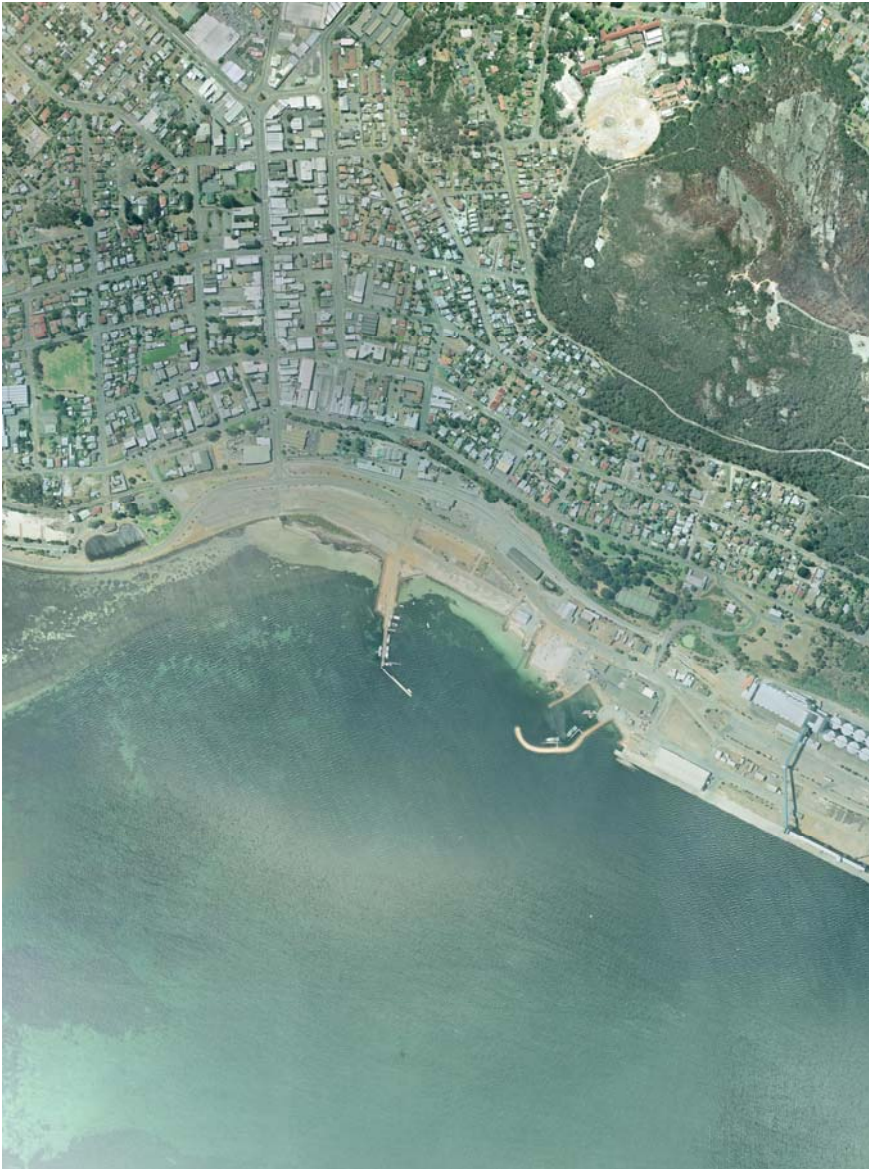


# Structure Plan Report

## INTRODUCTION

The Structure Plan prepared for the Albany Waterfront is intended to guide development in the Albany Foreshore Development Zone and is based on the Concept Plan accepted by the City in June 2005. The Concept Plan outlines the broad layout of the foreshore by identifying the size and shape of various potential uses without being prescriptive.

This Structure Plan provides more specific details and a framework for the proposed pattern of land use and development. It also guides the coordinated provision of infrastructure and services within the foreshore area to support the development and link with the CBD.



# Structure Plan Report

The Structure Plan differs from the Concept Plan in the following ways:



- The new marina has been enlarged to allow for 130 pens
- The proposed reactivation of the civic spine providing an at grade pedestrian link across the railway tracks has been deleted (the view corridor has however been maintained) and an overpass has been included
- The site for the Albany Entertainment Centre (AEC) has been repositioned eastward to abut the central Toll Place spine
- The site for the proposed hotel and serviced apartments has been repositioned to the eastern side of the central Toll Place spine and will accommodate between 120 to 145 hotel rooms plus 80 to 90 short-stay apartments, depending on room sizes
- Surface parking for approximately 792 cars has been provided
- Parking and landscape have been provided as a buffer to Princess Royal Drive

Rev

Rev

Rev: Number of potential hotel rooms and short-stay apartments has been increased from 125 hotel rooms and 50 short-stay apartments.

Rev: Number of car parking bays has been increased from 700 to meet the requirements of the increase in hotel and short-stay apartments

# Structure Plan Report



The site is one of the most outstanding in the region not only because of its stunning outlook but also due to its prominence from the CBD of Albany. From the site visitors are able to look across the picturesque expanse of Princess Royal Harbour to a horizon of undulating hills. This harbour view is at the very essence of the Albany experience and there are no other development sites in this dress circle location.

An added dimension of richness is brought to the site due to its close proximity to Albany's unique collection of historical buildings and streetscapes.



The site is very visible from the CBD and its surrounds due to the fact that Albany rises from its shore line. When the development is complete it will make a significant positive contribution to the overall townscape and act as a magnet drawing people to the foreshore area.

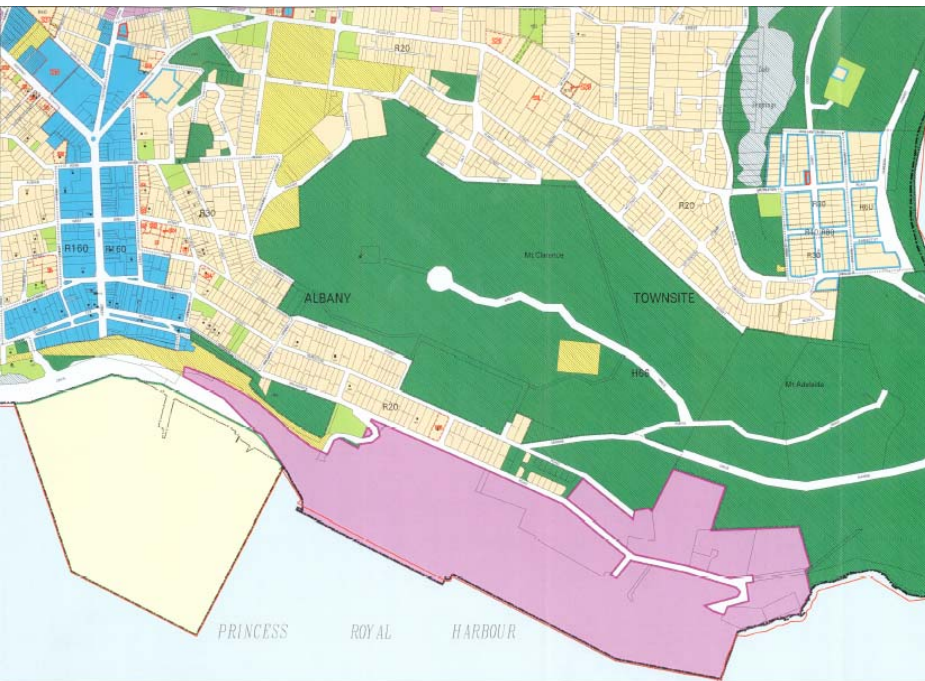


# Structure Plan Report

## STATUTORY PLANNING

The site lies within the Foreshore Development Zone. The northern boundary abuts Princess Royal Drive, the western boundary abuts the new Anzac Peace Park. (While the Anzac Peace Park is shown on the Structure Plan it is not part of the plan). The eastern boundary abuts the Albany Port. The southern boundary abuts and extends into Princess Royal Harbour and Location 7601 in Princess Royal Harbour forms part of this Structure Plan.

Appropriate land uses within this zone will be determined through the Structure and Precinct Planning process. The Scheme requires that a Structure Plan show the general distribution of land uses within the plan area. It requires that a subsequent Precinct Plan show the proposed use of all land within that precinct. Once adopted by Council, the Precinct Plan will become binding on development within that precinct.



# Structure Plan Report

## STRUCTURE PLAN

### Key determinants

There are four key determinants that are reflected in the Structure Plan:

- Port access - The project area is traversed by the two primary freight access routes to the Albany Port, these being Princess Royal Drive and the railway line. The Structure Planing recognises and protects this infrastructure to ensure that freight access to the port is not obstructed or impeded
- Short stay accommodation - Residential uses is restricted to short stay tourist accommodation only. Permanent residential development is not included
- Views and vistas - Views and vistas of Princess Royal Harbour are maintained through view corridors
- Public access to the foreshore - Public access to the foreshore is maintained



# Structure Plan Report

## Objectives

The principal planning and design objectives reflected in the Structure Plan are:

- To extend the Albany CBD to the waterfront via strong pedestrian, cycle and vehicular connections
- To create a viable mixed use development that attracts residents and visitors
- To establish an iconic focal point for the Albany Waterfront
- To provide a variety of public open spaces capable of use throughout the year
- To enable development to occur progressively and logically as demand increases
- To satisfy parking demands
- To enable local views to be maintained
- To provide an attractive new marina as the central element of the development
- To provide workable and efficient spaces for marine based activities





# Structure Plan Report

## Design Overview

The Structure Plan sits within a site envelope of approximately 17.3 hectares (inclusive of the marina, development sites along the new breakwater and fishing industry hard stand areas and boat trailer parking). The total building area, when fully developed will be in the order of 31,000m<sup>2</sup>.

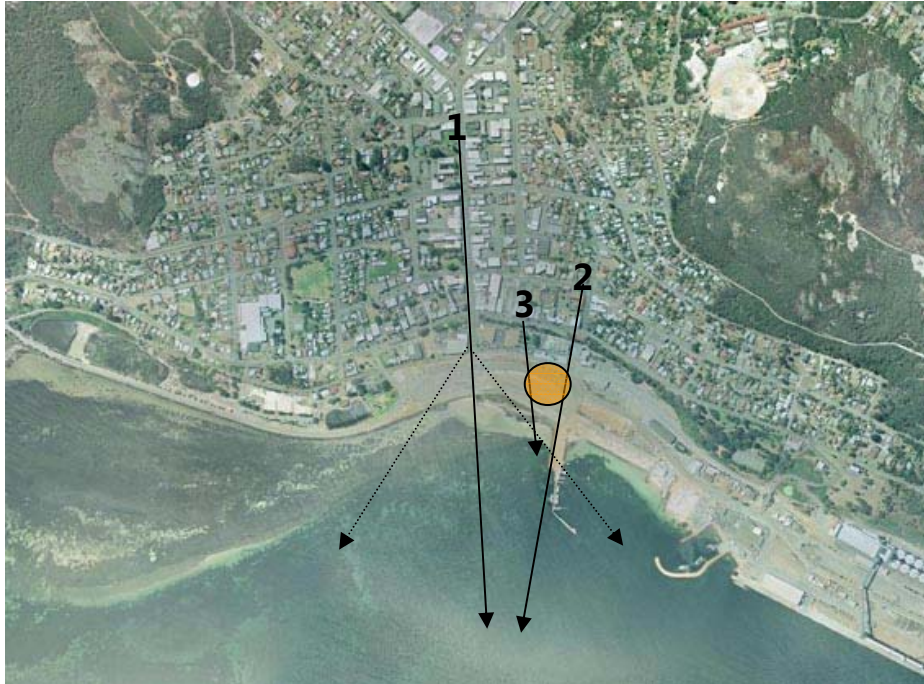
Rev



The proposed buildings shown in the Structure Plan are arranged in a linear formation along the existing sea wall edge to Princess Royal Harbour. They overlook the new 130 pen marina which reinforces the strong maritime character of the development. The buildings are set back from the sea wall edge sufficient to create a promenade linking one end of the development to the other. At the centre of the development are Toll Place and the protruding Town Jetty which acts as a central spine about which buildings can be grouped. On one side of the spine will be the new AEC. Facing it on the other side will be a hotel and serviced apartment complex.

Rev: The total building area has been increased from 23,000m<sup>2</sup> to 31,000m<sup>2</sup> due to an increase in potential area of hotel, short-stay apartments and mixed use buildings

# Structure Plan Report

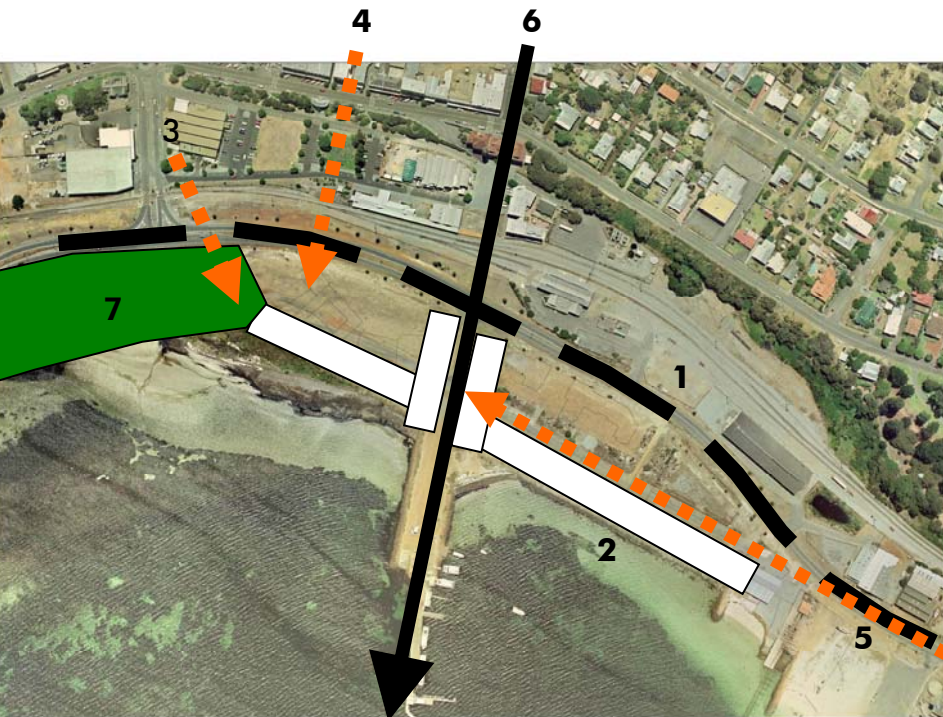


Preserving existing view lines has been a major determinant in the planning of the Albany Waterfront.

The York Street [1] and the Spencer Street [2] axes have been shown on this photograph. Both of these axes impact on the design because each defines a view which must not be blocked. At the base of York Street a view shed to Princess Royal Harbour occurs which begins to define the positioning of buildings on the western side of the site. Another viewing point occurs from the Rotunda in Stirling Terrace [3]. This view shed determines the maximum building heights to the western edge of the development. At the Rotunda the height difference between the foreshore and Stirling Street is approximately 6.5 metres thus limiting the height of any buildings in front of the Rotunda to two storeys. The position of the theatre fly tower, which is the highest element of the development, is shown with an orange circle.



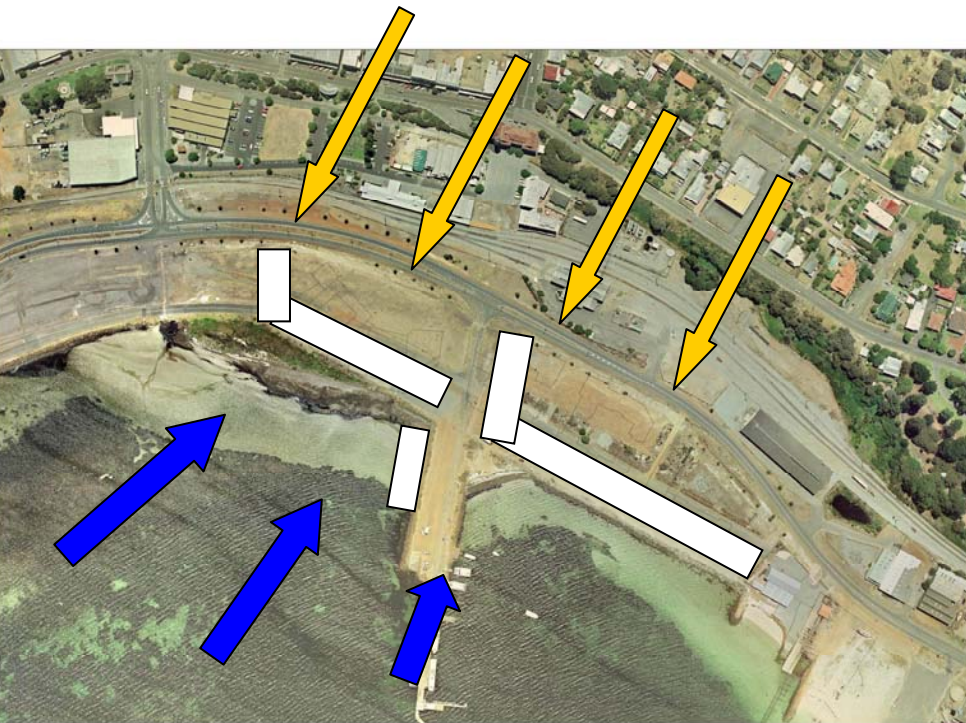
# Structure Plan Report



There are a number of topographic and contextual issues driving the site planning outcomes.

- Princess Royal Drive [1] is the main vehicular access to the Port and therefore a significant buffer zone is desirable
- The existing sea wall to Princess Royal Harbour is strikingly linear which suggests it be reinforced with a linear built edge [2]
- Pedestrian flow from the CBD is to be facilitated. This will occur along York Street [3] and across the new pedestrian bridge [4]
- Pedestrian flows from tourist-bearing cruise ships are to be facilitated [5]
- The Spencer Street view corridor should be reinforced [6]
- An appropriate interface with the Anzac Peace Park is to be provided [7]

# Structure Plan Report



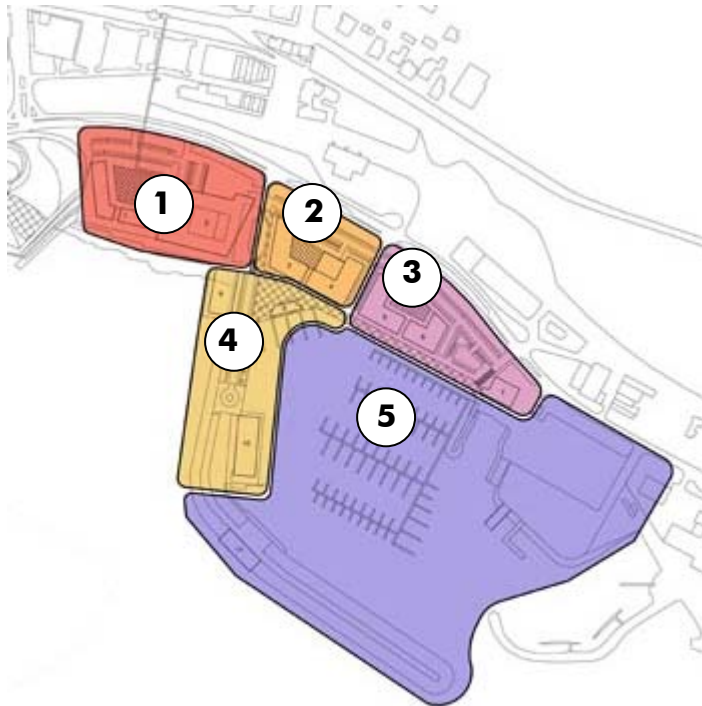
In addition to the above landside drivers, there is a very strong climatic determinant imposing itself on the design of this development. This figure shows the site being impacted by cold sea winds blowing across Princess Royal Harbour. A logical design response is to use buildings as a wind break and thus provide protected public spaces on the leeward side. The open spaces on the protected side of the buildings will enjoy warm northern sun. This provides a unique opportunity to design buildings with glass frontages on both sides. Building users can enjoy the dramatic harbour view while at the same time they can enjoy the warmth of the northern sun.

# Structure Plan Report

## Precincts

The Structure Plan comprises the following five precincts:

- Entertainment Precinct [1] is to the western side of the Toll Place spine comprising the AEC, mixed use retail and commercial, public open space
- Accommodation Precinct [2] is to the eastern side of the Toll Place spine comprising two sites for a hotel and short stay/serviced apartments
- Commercial Precinct [3] is to the east of the Accommodation Precinct comprising maritime focussed mixed commercial and retail uses with capacity for short-stay apartments on the third floor
- Town Jetty Precinct [4] is centrally located comprising mixed use maritime, retail and commercial uses
- Harbour precinct [5] comprises maritime based light industrial uses complementing the uses of the Port and of Albany and the Town Jetty. This precinct also includes the marine, fishing industry hard stand, boat ramp and trailer parking.



Rev

Rev: Inclusion of possible short-stay apartments on level three in the Commercial Precinct

# Structure Plan Report

## Uses

The Structure Plan indicates approximate building footprints and areas within the five precincts as tabled below. The areas and footprints are approximations only. The intent is to demonstrate the likely built form characteristics in each precinct. The Precinct Plan will follow these approximations when developing the design guidelines.

Rev: GFAs for the hotel, short-stay apartments and mixed use buildings has been increased due to increased building heights

Rev: Short-stay apartments added as suitable use in commercial precinct

Precinct	Uses	Approximate GFAs (m2)
Entertainment Precinct	Mixed use commercial and retail	2,200
	Albany Entertainment Centre	3,300
Accommodation Precinct	Hotel	10,500
	Short stay serviced apartments	6,800
Commercial Precinct	Mixed use commercial, retail and short-stay apartments	6,750
Town Jetty Precinct	Retail (Café, restaurant, shops)	1,800
Harbour Precinct	Concession offices	200
	Duyfken Workshop	660
	Boat ramp and trailer parking	6,500
	Fishing industry hardstand	5,500
	Marina for 150 boats	
	Fishing platforms	60

Rev

Rev

# Structure Plan Report

## Parking

The vehicle parking standards applicable to the Foreshore Development zone will be determined as part of the Precinct Planning process. The parking standards will generally be as tabled below.

Use	Parking bays /m2 NLA
Mixed use	1 / 30m2
Retail	1 / 15m2
Serviced apartments	1 / unit
Hotel	1 / room 1 / 3m2 in bar areas 1 / 4m2 in eating areas
Entertainment Centre	1 / 4 seats the facility is designed to accommodate
Marina	1 / 5 pens

Rev: Car parking bays increased by 92 to meet the requirements of additional hotel rooms and short-stay apartments

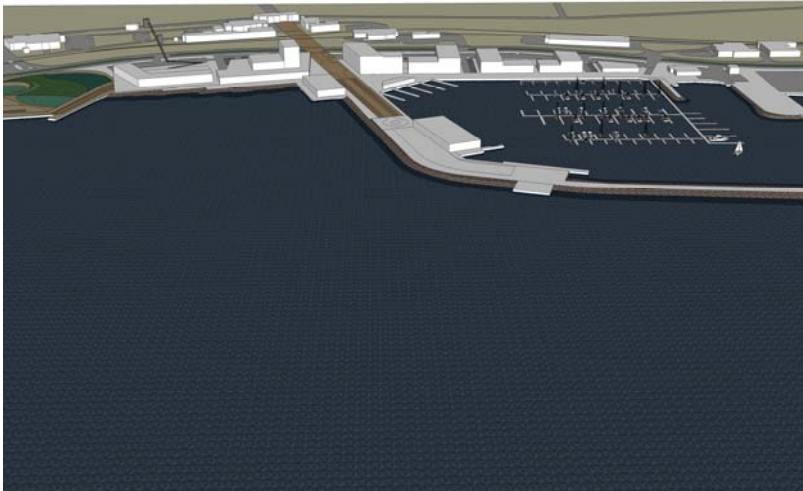
The Structure Plan indicates area for 792 parking bays on site with additional overflow being accommodated after dark in the boat trailer car park.

Rev

# Structure Plan Report

## Built form

The Structure Plan shows a number of buildings in each precinct. These building footprints are approximate, their main purpose being to show the relationship between future buildings, car parking, public open space and site limitations.



Height limits have been placed on buildings within each precinct as tabled below:

Precinct	Uses	Heights
Entertainment Precinct	Mixed use commercial and retail	2 storeys with potential to use roof volume
	Albany Entertainment Centre	Variable with fly tower being limited to 7 storeys
Accommodation Precinct	Hotel	5 storeys
	Short stay serviced apartments	6 storeys
Commercial Precinct	Mixed use commercial, retail and short-stay apartments	3 storeys
Town Jetty Precinct	Retail (Café, restaurant, shops)	1 storey
	Concession offices	1 storey
Harbour Precinct		1 storey

Rev

Rev: Remove reference to potential use of roof volume and increase height of short-stay apartments to 6 storeys and mixed-use to 3 storeys



# Structure Plan Report

The overall character of the buildings within the development will be diverse mainly due to the range of uses. The AEC will, because of its mass and public function, be quite different to its neighbouring buildings.



The new AEC has created a distinct landmark in the Waterfront development due to its striking roof lines. Other buildings in the development should therefore minimise the use of large, dominant roofs to ensure the primacy of the AEC. Low pitched roofs would be suitable. The use of green roofs is encouraged. On single storey buildings, low-pitched pitched roofs would be suitable.

Rev

An important design principle adopted in the Structure Plan has been to avoid facing the backs of buildings to Princess Royal Drive. All buildings within the development will have two frontages and design guidelines will be produced in association with the Precinct Plan which will determine the appropriate ways for buildings such as the hotel to accommodate their services areas without jeopardising the double fronted principle. This is particularly important when considering service access to the AEC.

Rev: Reference to the use of large pitched roofs has been removed and replaced with advice to reduce impact of roof forms.

# Structure Plan Report

## Public spaces

The three major public open spaces in the development are:

- Entertainment forecourt [1]
- Waterside promenade [2]
- Toll Place [3]



Entertainment forecourt – this is a large open space protected from the prevailing weather and facing north. It is protected by built form edges to the south. This area also acts as the termination point for the new pedestrian bridge and therefore provides a generous movement space allowing large numbers of people to enter and leave the development.

# Structure Plan Report



The forecourt can have several useful functions including:

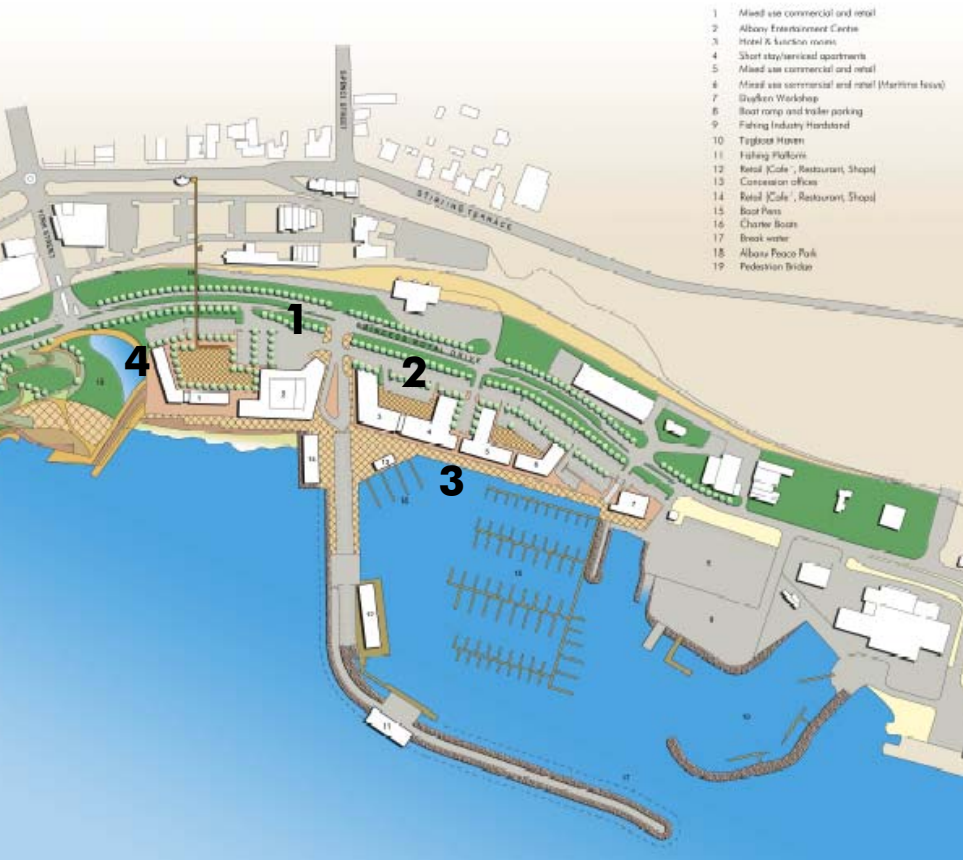
- Open space for family and social gatherings
- A large pre-performance gathering space associated with the AEC
- A potential 'market-style' area which could be used for community based markets

Waterside promenade – this is the key open space in the development as it provides the interface between the water and land-based facilities. The promenade is a linear open space with a generous width. It is envisaged that the promenade will be a highly activated part of the development facilitated mainly by the ground level retail activities associated with the various precinct buildings.

# Structure Plan Report

At the water's edge the promenade will step down to enable people to be more closely related to the water. The promenade fronts the new marina which further enhances the activity and interest of the place.

Toll Place – this is the open space located between the AEC and the proposed hotel/serviced apartments. It could be seen as the principal entrance point to the development coming off Princess Royal Drive. It not only provides a main entry to the site but accommodates the pedestrian and vehicular movements associated with the main entries to the theatre and hotel. This space also reinforces the harbour vista and leads visitors to the various activities located along the new break water.



## Landscape

The important landscape areas within the development are:

- Princess Royal Drive [1]
- Internal car parks [2]
- Waterside Promenade [3]
- Interface between the Waterfront and the Peace Park [4]

# Structure Plan Report

Princess Royal Drive - the verge treatment to Princess Royal Drive will be landscaped to provide a softer edge and to create an appropriately scaled interface between the development and Princess Royal Drive.

Internal car parks - will be landscaped to soften the spaces and to break down the expanse of parking. Design guidelines will be prepared to ensure the selection of trees appropriate to the environment and adjacent buildings.

Waterside Promenade - will be treated with primarily hard landscape in the form of paving, artwork, bollards, lighting and edge treatments suitable to endure the marine environment while creating an exciting visual experience for visitors.

Interface between the Waterfront and the Peace Park – this is an important aspect of the development and will be detailed during the Precinct Planning process to ensure an appropriate union occurs between the two spaces.

All landscaping proposals in the following Precinct Plans will reflect the reality of the marine environment in which living landscape must grow.

# Structure Plan Report

## **Pedestrian movement and access**

The two principal pedestrian patterns linking the Waterfront to the CBD are:

York Street link – this link occurs at the end of York Street where pedestrians will cross Princess Royal Drive and enter the Waterfront development where the development and the Peace Park abut. From this point pedestrians will access the promenade. One of the important features of this link is the presence of the AEC. As pedestrians arrive at the end of York Street they will be greeted by the presence of the AEC which will act as a strong attractor and draw pedestrians to the Waterfront.

Pedestrian bridge link – this link facilitates pedestrian flow from Stirling Terrace to the Waterfront. The bridge enables pedestrians to cross above the railway and Princess Royal Drive. The Stirling Terrace end of the walkway is at grade and suitably graded ramps at the Waterfront end ensure accessibility for disabled pedestrians.

In addition to the links with the CBD there are important pedestrian links within the development. These links occur between buildings in the

# Structure Plan Report

development and enable pedestrians to move from the car parks facing Princess Royal Drive to the waterfront promenade. Some areas of parking are located in close proximity to the promenade to provide convenient access for people with penned boats.

The promenade extends to include the existing Duyfken workshop thus ensuring that this facility and the maritime based uses beyond are conveniently linked.

## **Traffic**

The following traffic management principles have been adopted in the Structure Plan:

- All car parking is located within the development and in close proximity to the facilities.
- The number of entry points from Princess Royal Drive have been minimised

# Structure Plan Report

- The distance between entry points from Princess Royal Drive have been maximised in accordance with Option 1 in the Traffic Report titled "Albany Waterfront Traffic Assessment" by Riley Consulting, attached in Appendix 1
- Access and turning circles for large articulated vehicles servicing the AEC have been considered in the intersection designs shown in the Traffic Report

The Precinct Plan will acknowledge the detailed proposal for traffic management outlined in the Traffic Report.

## Services

The following major services will be provided in the following manner:

Sewer - This development will be served via an existing 300 diameter gravity sewer in Princess Royal Drive, which is immediately adjacent to the project. This sewer discharges north along York Street to the existing pumping station north of the rail. Internal sewer will be via a gravity extension of this sewer.



# Structure Plan Report

Water - Water supply for the project will come from the existing 150 diameter water main in Princess Royal Drive. Some realignment of this will be required as part of the construction.

Underground Power - There is no capacity in the existing adjacent power system to handle this project. Underground Power will subsequently be arranged for the project via Western Power extensions and upgrades of the existing infrastructure some 10 km to the North. Internal distribution will be then achieved by standard practice.

# Structure Plan Report

## Subdivision

It is proposed to subdivide the Waterfront development site. A notional subdivision layout is shown below to demonstrate the following subdivision principles:



- Establish a site area for the Albany Entertainment Centre which includes the Entertainment Forecourt and frontage to Princess Royal Drive
- Establish a number of sites for private commercial development (hotel, mixed use buildings) fronting car park along Princess Royal Drive
- Reserve an area for public access
- Establish site areas for commercial development along the Town Jetty spine (services easements and vehicle access within public use reserve)
- Establish areas for public parking



## **PRECINCT PLANS**

INTRODUCTION	<b>28</b>
VISION	<b>29</b>
OVERALL DESIGN OBJECTIVES	<b>30</b>
STATUTORY REQUIREMENTS	<b>31</b>
PLANNING AND DESIGN PRINCIPLES	<b>33</b>
PRECINCTS:	
Entertainment Precinct	<b>56</b>
Accommodation Precinct	<b>57</b>
Commercial Precinct	<b>58</b>
Town Jetty Precinct	<b>59</b>
Harbour Precinct	<b>60</b>
LAND USE TABLES	<b>61</b>
IMPLEMENTATION	<b>65</b>
ADDENDUM 1	<b>66</b>
Accommodation and Commercial Precincts	

Revised in July 2011 to include changes involving an increase in area of the hotel, short-stay and mixed-use components of the Albany Waterfront

# Precinct Plan Report



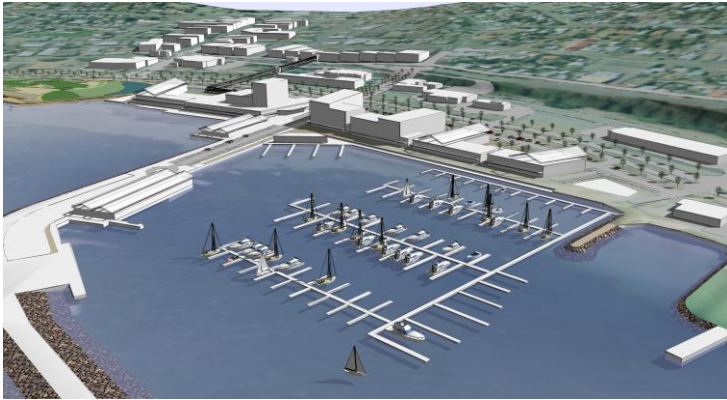
## INTRODUCTION

The Precinct Plan identifies five distinct precincts within the Albany Waterfront area and outlines a vision, objectives and detailed planning and design guidelines for each. The Precinct Plan represents a logical extension of the endorsed Concept Plan and Structure Plan and also takes into account the comments received during community consultation.

The preparation of a Precinct Plan for the Albany Waterfront is a vital part of the planning process for the locality. By outlining the land use, zoning and development controls that apply to each precinct, the vision and objectives for each can be realised.

Council is to have regard for the Precinct Plan when assessing applications, and in doing so, it will guide Council's decision making for development applications and infrastructure provision within the area.

# Precinct Plan Report



## VISION

**The Albany Waterfront will actively link the CBD to Princess Royal Harbour by providing a people-focused development with a strong sense of vibrancy and excitement with a unique combination of entertainment, accommodation and mixed-use buildings facing a wide waterside promenade overlooking an active public marina.**



# Precinct Plan Report



## OVERALL DESIGN OBJECTIVES

- Overall theme of development is to reflect its maritime context.
- Provision of unhindered port access via Princess Royal Drive.
- Provision of appropriate pedestrian flows from the CBD and within the development.
- Provision of adequate onsite parking and vehicle access.
- Provision of a safe and secure environment for all members of the community at all times of the day and night.
- Ensure building scale, materials and colours complement the Stirling Terrace interface with the Albany CBD.
- Provision of an attractive, open and consistent streetscape to Princess Royal Drive.
- Provision of an active edge to the waterside Promenade.
- Design quality that reflects the significance of this precinct as a key destination in Albany and the broader region.

# Precinct Plan Report

## **STATUTORY REQUIREMENTS**

The requirement for producing Precinct Plans is contained in Town Planning Scheme No. 1A.

Upon adoption, all development is required to comply with the Precinct Plans and any development that is deemed not to be consistent with the intention of the Precinct Plans may be refused by Council.

In accordance with Appendix III of the Scheme all development standards applicable within this zone are to be in accordance with the adopted Structure and Precinct Plans.

In accordance with TPS 1A requirements, Appendix IV contains numerous development standards for the types of uses which may be accommodated within the Waterfront development area, including parking, minimum lot area, plot ratio, boundary setbacks and landscaping, although the development standards contained within the adopted Precinct Plan take precedence under the provisions of Clause 4.1.

# Precinct Plan Report

The Structure and Precinct Plans therefore contain all the relevant statutory planning requirements for the Foreshore Development area, including all use permissibility and development standards.



# Precinct Plan Report

## PLANNING AND DESIGN PRINCIPLES

A number of planning and design principles are to be adhered to in all proposed developments in the Albany Waterfront. Proponents submitting development applications to the City of Albany must demonstrate a sound understanding of these principles. The following section outlines these planning and design principles.

1. Materials
2. Setbacks
3. Elevations
4. Parking
5. Roofscape
6. Active Frontages
7. Northern Orientation
8. Signage
9. Pedestrian Access and Bicycles
10. Awnings, Canopies and Balconies
11. Levels and Height Management
12. Public Art
13. Vehicle Management
14. Landscape
15. Resource Efficiency
16. Colours
17. Services
18. Traffic Noise
19. Safety and Security
20. External Lighting
21. Environment
22. Marina and Water's Edge
23. Site Facilities
24. Short-Term Residential Site Planning
25. Site Planning

## **1.0 MATERIALS**

- 1.1 Building enclosure materials will generally complement the quality of those used in the Albany CBD, in particular the buildings fronting Stirling Terrace.
- 1.2 Finishes and materials used for external walls will be of a quality sufficient to reflect the important public nature of the development.
- 1.3 Examples of suitable finishes are faced limestone or painted render. Rendered and painted concrete panel is acceptable provided its detailing, particularly at pedestrian level, is appropriately refined.
- 1.4 Face brickwork, corrugated steel and timber are suitable for feature panels to external walls however these materials should not represent more than 10% to 15% of the total elevation.
- 1.5 Corrugated steel sheeting is not a suitable wall material but is acceptable in gables and spandrels.
- 1.6 Paving materials will be of a quality usually associated with high traffic civic precincts.
- 1.7 Materials and detailing will ensure longevity in a harsh marine environment.
- 1.8 Roof tiles and unpainted or 'brilliant' white metal sheet roofing materials are not acceptable.
- 1.9 Use of highly reflective glazing will not be permitted. Samples of any reflective or tinted coatings to glazed areas will need to accompany any development application and will be to the satisfaction of the COA.

## **2.0 SETBACKS**

- 2.1 All buildings will be set back a minimum of 25m from the Princess Royal Drive road reserve boundary.
- 2.2 Buildings fronting the Promenade may have a zero setback from the Promenade.

# Precinct Plan Report

2.3 The AEC and adjacent hotel building shall generally be built up to Toll Place. The actual location of these buildings in relation to Toll Place shall be determined by the spatial requirements associated with vehicle set down areas, road widths and pedestrian paths.

2.4 At the boundary between the Albany Peace Park and the Albany Waterfront Development, the buildings will be set back from the boundary sufficient to enable an unobstructed 5.0m minimum width paved pedestrian way linking the Promenade to Princess Royal Drive. The boundary treatment shall include complementary landscaping to interface harmoniously with the Albany Peace Park.

## 3.0 ELEVATIONS

3.1 The Albany Waterfront Development effectively has two main frontages and each must be treated appropriately.

3.2 Elevations facing Princess Royal Drive are to be regarded as main street elevations.

3.3 Elevations facing the Promenade and Toll Place are to be treated as building frontages displaying the quality of detailing appropriate for the public nature of the location.

3.4 Service access to the AEC shall be designed to prevent it appearing as 'the rear' of the AEC.

3.5 Air conditioners must not feature on any walls facing Princess Royal Drive, the Promenade or Toll Place and should be screened from the public realm to the satisfaction of COA.

3.6 Elevations of all building while being contemporary will complement the general characteristics of buildings in the CBD and adjacent environs without attempting to copy or replicate their historical architectural style.

# Precinct Plan Report

3.7 Suitable characteristics that could be used in combination include:

- Front facades being as continuous as possible
- Well defined and recessed (750mm minimum) front entries to all entries facing the Promenade
- Construction to lot boundaries facing the Promenade
- Provision of linear cantilevered awnings
- Creating a 'building base' or plinth using a change of material, colour or multiple horizontal banding at the ground floor level
- Verge and eaves overhangs sufficient to create strong shadow lines
- Windows, other than window walls and large shop fronts, to be taller than they are wide and well recessed
- Horizontally projected banding at logical intervals (floors, window heads, sills) to increase the continuous linearity of the façade
- Openings, windows, columns where possible to be logically and rhythmically arranged
- Windows at ground floor level along all active frontages to be at least 2.4m high.



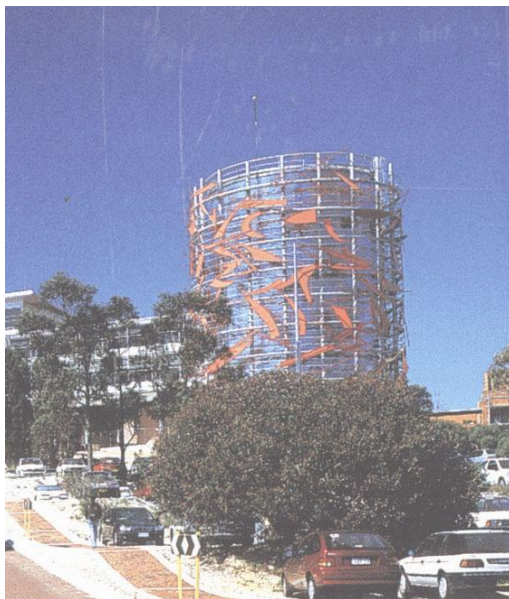
These images of buildings in Stirling Terrace and the adjacent railway station are examples of buildings exhibiting some of the design characteristics which should be reflected in the new Albany Waterfront buildings. The simple yet strong roof forms, linearity reinforced by continuous awnings, subtle background colours, regularity of fenestration arrangement are compositional elements well used in these buildings.



These images of buildings within the same overall precinct are examples of buildings exhibiting some of the design characteristics which should NOT be reflected in the Albany Waterfront buildings. Roof pitches are too steep, elevations are too eclectic and complex, background colours are too intense, signage is inappropriate.

# Precinct Plan Report

3.7 The fly tower associated with the AEC will be a very visible component and will provide a central focus for the development. The fly tower shape and form is derived from functional needs this element should be treated in an inspirational way. This could be achieved in a number of ways including the use of transparent cladding materials, creative illumination, sloping of the roof, attachment of creative and inspiring three dimensional art works, colour and so forth.



This image shows how a tall functional building element without windows can be creatively treated.

## 4.0 PARKING

4.1 The Albany Waterfront development when complete will generate a need for 792 car bays. The Precinct Plan indicates how these car bays are to be distributed across the development. The car parking plan for each site is to be adhered to in order to satisfy the overall parking requirements.

Rev

4.2 The parking requirements for specific uses shall be determined in accordance with the following table:

Use	Parking bays/m2 NLA
Mixed Use	1/30m2
Retail	1/15m2
Serviced Apartments	1/unit
Hotel	1/room 1/3m2 in bar areas 1/4m2 in eating areas
Entertainment Centre	1/4 seats the facility is designed to accommodate
Marina	1/5 pens

Rev: Increase car bay numbers from 700 to 792 to meet requirements of additional building area associated with the hotel and short-stay apartments.

# Precinct Plan Report

4.3 Reciprocal rights of access will be required for parking areas fronting Princess Royal Drive.

4.4 Dimensions and layout of parking areas will be in accordance with AS 2890 "Parking facilities-Off-street car parking". One parking space in every ten shall be planted with trees or shrubs and these bays will be included as parking and not landscaping.

4.5 Joint use of parking areas will be encouraged.

4.6 No basement or part basement parking is permitted.

## 5.0 ROOFSCAPE

5.1 Due to Albany's hilly terrain parts of the City centre will look down upon the Albany Waterfront. The roofscape is therefore considered to be a 'fifth elevation' and its design will be treated accordingly.

5.2 Simple roof forms complementing the linearity of the buildings are desirable.

5.3 Roofs on single storey and two storey buildings should have substantial overhangs creating strong shadow lines. Minimum 800mm to verges and 1200mm eaves is suggested.

Rev

5.4 Strutting of overhangs is not encouraged.

5.5 Low pitch roofs ( $<10-15^{\circ}$ ) are to be used on taller buildings such as the hotel and short-stay apartments. Consideration should be given to the use of green roofs on these buildings.

Rev

5.6 Roof pitches on lower buildings should not be steeper than the roof pitch on the adjacent railway station building.

Rev

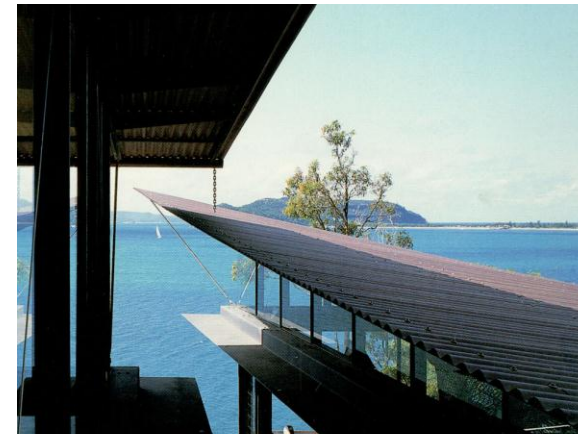
5.7 The visual depth of roofs at all overhangs will be minimised.

Rev: Pitched roofs with eaves restricted to single and perhaps two storey buildings.  
Low pitch roofs required for hotel and short-stay apartments.  
Green roofs are encouraged.

# Precinct Plan Report

- 5.8 No plant and equipment associated with air conditioning will be mounted on the roof of any buildings in the development.
- 5.9 Aerials, antennas, masts, may be roof mounted provided it is satisfactorily demonstrated that they are an integral part of the design and contribute to the character and interest of the development. Mounting of large dishes on roofs is not permitted.
- 5.10 If buildings with lifts require a lift overrun then it should be satisfactorily demonstrated that the element containing the overrun does not compromise the quality of the 'fifth elevation'. It is preferable to contain any overrun within the roof space.

Rev: Low pitch, parapet roof illustration added for hotel and short-stay apartments.  
Pitched roof illustration suitable for single or two storey buildings



The image above illustrates strong, simple, and 'thin' roof forms that would be suitable for single and perhaps two storey developments.



The image above illustrates the type of roof treatment expected for the hotel and short-stay apartments

# Precinct Plan Report

## 6.0 ACTIVE FRONTAGES

- 6.1 The pedestrian-level experience along the Promenade is to be stimulating and vibrant. Blank walls at this level are not permitted. The upper levels of the hotel and short-stay serviced apartments, where they face the Promenade should include functional size balconies.
- 6.2 Elevations facing the Promenade will have a minimum of 80% windows at pedestrian level.
- 6.3 All designated active frontages will be well illuminated.
- 6.4 Window sill heights along all active frontages will generally not be more than 200mm above floor level. Some areas may have small sections of higher sills provided the overall interior/exterior sight lines are not compromised.
- 6.5 Where cafes, restaurants, coffee shops and the like front the Promenade and Toll Place the capacity to open up these facilities to provide an alfresco area will be strongly encouraged.

- 6.6 In alfresco areas along the Promenade wind mitigation devices such as glass screens are to be incorporated thus preserving views to Princess Royal Harbour.
- 6.7 Unscreened air conditioning plant is not permitted at ground level along any designated active frontages.



This image shows how a glass frontage at pedestrian level can offer so many opportunities to visually link internal and external areas. Note also the strong horizontality which is a desirable characteristic of buildings in the Albany Waterfront.



# Precinct Plan Report

## 7.0 NORTHERN ORIENTATION

- 7.1 The north facing side of all buildings is protected from the prevailing southerly winds arising from Princess Royal Harbour. Designing buildings with ground floor spaces that flow into this protected northerly space is strongly encouraged.
- 7.2 The inclusion of useful north-facing balconies in buildings associated with the Accommodation Precinct is strongly encouraged.

## 8.0 SIGNAGE

- 8.1 Pylon signs erected by individual land owners will not be permitted. Combined signage themed to complement the development is acceptable provided it is designed as part of an entry statement located at the intersection of Princess Royal Drive and Toll Place.
- 8.2 If a building has numerous tenants, consolidated signage is preferred.

- 8.3 All building signage will be of a high standard and generally not exceed 10% of the building wall to which it is fixed, except for larger walls associated with the hotel and serviced apartments where signage will not exceed 5%.
- 8.4 Signage suspended below awnings, canopies or balconies or cantilevered will have a minimum clearance above footpath level of 2.75m.
- 8.5 Illuminated signage (not pulsating or flashing) is preferred to externally lit signs.
- 8.6 Signage will be maintained in good order for the life of the development.
- 8.7 Signage erected above rooflines will not be permitted.

# Precinct Plan Report

8.8 Windows facing Princess Royal Drive, the Promenade, and Toll Place shall not obscure more than 25% of their area with painted or installed signage.

## 9.0 PEDESTRIAN ACCESS AND BICYCLES

9.1 North-south pedestrian access points are to be permanently provided in the locations indicated on the Precinct Plan. These points provide important linkages for pedestrians to access the Promenade.

9.2 Maximising pedestrian access and circulation throughout the whole development will be a priority.

9.3 Consideration generally must be given to designing access around the Waterfront suitable for people with impaired mobility. In particular, changes in level must include provision of ramps as well as steps.

9.4 All tourist, residential, commercial, entertainment and mixed-use developments must take account of disabled access as required by the Building Code of Australia.

9.5 Clearly defined pedestrian pathways will be provided from the base of the pedestrian bridge to the Promenade.

9.6 In areas where there is a potential conflict between pedestrian and vehicular movement consideration must be given to ensuring the safety of pedestrians. Bollards and railings designed to complement the marine theme of the development are a suitable management device.

9.7 Provision should be made for secure bike parking and racks should be provided in appropriate locations within the developments.

## 10.0 AWNINGS, CANOPIES AND BALCONIES

10.1 Pedestrian pathways along building edges facing the Promenade, Toll Place and Princess Royal Drive will be protected by continuous cantilevered canopies. Provision and maintenance of canopies and awnings is the responsibility of the building owner.

# Precinct Plan Report

- 10.2 The use of balconies associated with the hotel and short stay serviced apartments is encouraged.
- 10.3 The minimum height for a canopy (or balcony) overhanging a public pedestrian access way will be 2.75m above the access way or footpath.
- 10.4 Where zero setbacks occur, in particular along the Promenade, cantilevered awnings and balconies may project up to 2.5m beyond the building face.
- 10.5 Balcony balustrades comprising solid materials such as brick or concrete or other obscure materials are generally not permissible and all balustrade material is to be capable of withstanding the harsh marine environment.
- 10.6 Balustrade detailing reinforcing the maritime context is encouraged.
- 10.7 Large scale canopies associated with the vehicle drop off areas adjacent to the AEC and Hotel are to be generally transparent

to prevent undue shadowing of this area and the use of support columns is to be minimised.

- 10.8 Canopies are to read as thin, lightweight elements.

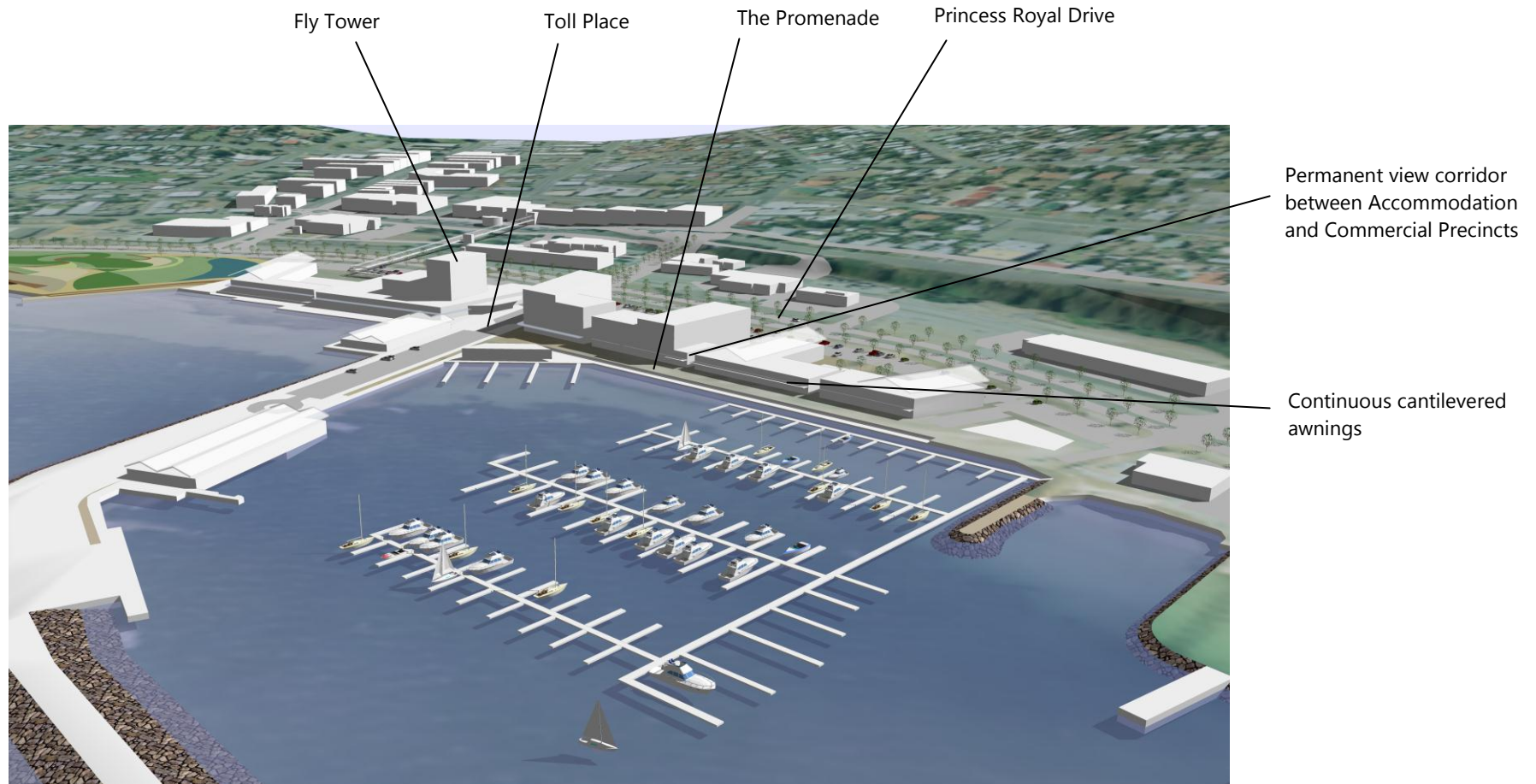
## **11.0 LEVELS AND HEIGHT MANAGEMENT**

- 11.1 In order to maximise viewing potential from Stirling Terrace, the tallest elements of all buildings will be orientated approximately north-south.
- 11.2 The view corridor aligning with Spencer Street must not be obstructed.
- 11.3 The fly tower associated with the AEC is the highest element permitted in the development and will be positioned to minimise interruption of views from Stirling Terrace. Its preferred position is shown on the Precinct Plan.

# Precinct Plan Report

- 11.4 Maximum permissible building heights progressively reduce both in an east and west direction away from the AEC's fly tower.
- 11.5 Ground floor ceiling heights throughout the development should generally be higher than minimum to reflect the ceiling heights associated with historical buildings in Albany. Minimum ceiling heights in the order of 3.0 to 3.5m are suggested. The AEC will have a ground level ceiling height commensurate with its public function.
- 11.6 Floor to floor levels, excluding the ground floor, in the Hotel and short-stay serviced apartment buildings should be limited to 3.0m.
- 11.7 Floor to floor levels, excluding the ground floor, in mixed use buildings should be limited to 3.5m.
- 11.8 Finished ground floor levels for all buildings fronting the Promenade and Toll Place will be no more than 200mm above the finished level of external paving.
- 11.9 All buildings will be constructed with a minimum ground floor level of 2.5m AHD.

# Precinct Plan Report



The image above demonstrates the height management principles associated with the Albany Waterfront. Note in particular: steady reduction, in an easterly and westerly direction, of building height from the highest allowable point at Toll Place; the higher portions of buildings orientated normal to Stirling Terrace; lower sections parallel to the Promenade; the position of the AEC fly tower; breaks in buildings fronting the Promenade to provide view corridors to the marina.

# Precinct Plan Report

## 12.0 PUBLIC ART

- 12.1 Public art will be provided as part of the development of each site in the Albany Waterfront.
- 12.2 Permanent public art work should be durable and complement the maritime theme.
- 12.3 Public art will be appropriate for use in an urban public space and it should not compromise public safety.
- 12.4 Public art in the Albany Waterfront development may be freestanding objects located on the ground, on a support structure, on a wall or suspended in the air.
- 12.5 Opportunities for public art include: murals, tiles, mosaics or bas-relief to walkways and walls; unique, artistically devised elements which also have a functional role eg bollards, gates, light poles, bench seating, drinking fountains.

## 13.0 VEHICLE MANAGEMENT

- 13.1 Parking is to be located in the positions indicated on the Precinct Plan.
- 13.2 Provision must be made for large articulated vehicles to enter from Princess Royal Drive, service the delivery dock area associated with the AEC and return in a forward direction to Princess Royal Drive.
- 13.3 Provision must be made for coaches to enter Toll Place, unload passengers and return in a forward direction to Princess Royal Drive.
- 13.4 General vehicle access to the Promenade is not permitted however emergency vehicles, particularly fire fighting vehicles must be able to access the southern sides of all buildings fronting the Promenade.
- 13.5 The boat trailer hardstand area and the Fishing Industry Hardstand area will be designed to ensure the safe and efficient manoeuvring and parking of vehicles and trailers.

# Precinct Plan Report

## 14.0 LANDSCAPE

14.1 Design of landscape in the Albany Waterfront will:

- Complement the function of the streets
- Reinforce desired traffic speed behaviour
- Be appropriately scaled relative to both street reserve widths and the building bulk and scale
- Take into account the image and role of the development, solar access requirements, soils, selection of appropriate species and services
- Assist where possible in micro climate management, particularly on the northern side of the development
- Enhance pedestrian comfort and safety
- Include 'water wise' strategies that assist to reduce long-term water consumption

14.2 A continuous two metre wide landscape strip will be provided between the Princess Royal Drive road reserve boundary and car parking areas facing Princess Royal Drive.

14.3 Trees planted in the car park aisles may be protected with kerbed landscape strips or tree grates and bollards.

14.4 Trees planted in the car park will be installed as mature specimens no less than three metres in height. Large trees are preferred in order to allow canopies to be managed in such a way that they are elevated and allow visibility to building facades and signage.

14.5 Materials and detailing associated with paving in important public areas such as the Promenade, the Public Open Space adjacent to the AEC and Toll Place will make strong reference to the materials used in York Street. This may be achieved through the use of stone kerbs and gutters, stone walls, and carefully detailed bollards and rails.

14.6 Landscaping proposed on the deck carpark in the Accommodation Precinct should be planted with local species including ground covers, shrubs and trees. This landscape treatment should form part of the overall stormwater treatment system for the development and it should include elements such as paths, seating and lighting and other treatments necessary to provide a suitable amenity space for hotel guests and patrons.

Rev 17/10/11

# Precinct Plan Report

## 15.0 RESOURCE EFFICIENCY

- 15.1 All buildings are to conform to the energy efficiency requirements of the Building Code of Australia.
- 15.2 Building envelopes and internal layouts will be designed to minimise energy consumed for heating, cooling and artificial light where:
- Window design facilitates good thermal and daylight performance
  - Building materials and insulation assist in providing comfortable thermal conditions
  - Air movement within buildings is designed to provide comfortable thermal conditions and appropriate air quality
  - Building materials, appliances and fuel sources are selected to minimise energy requirements and greenhouse gas emissions.

15.3 Building services are designed to minimise energy and resource use in the following way:

- Maximise use of natural light
- Utilise energy efficient lighting control systems, fittings and appliances
- Utilise energy efficient motors and equipment
- Maximise use of natural ventilation
- Utilise energy efficient air conditioning and mechanical ventilation systems and controls where appropriate
- Minimise water use and waste
- Utilise energy efficient hot water systems
- Utilise water efficient taps and fittings.

15.4 All building services are to be designed and maintained to minimise energy use over the life of the development.

15.5 Building designs should maximise use of renewable energy sources and utilise fuels that minimise greenhouse gas emissions.

15.6 Landscape design should assist microclimate management to conserve energy and water.



# Precinct Plan Report

## 16.0 COLOURS

16.1 The Albany Waterfront is to be a harmonious grouping of buildings each with a different role and potentially a different character. A limited pallet of external colours and building materials is to be used to ensure building harmony eventuates.

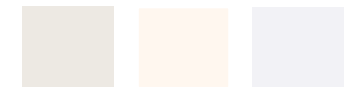
16.2 The following colour-related principles and guides will apply:

- Colours of external walls and building structure are not to be dominant.
- Roof colours will closely relate to wall colours. Strong coloured roofs (red, green, blue) will not be acceptable unless the proponent can satisfy Council that it makes a positive contribution to the whole development.
- Highlighting colours are strongly encouraged to add vibrancy and contrast. These colours should be strong warm colours (red, red-orange, orange, yellow-orange) or strong cool colours (yellow-green, green, blue-green, blue, blue-violet).

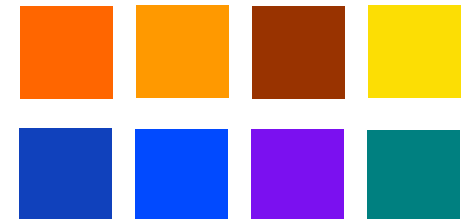
- Highlighting colours could be applied to screens, attached panels, spandrels, gables, selected integrated panels, coloured glass, and vertical support elements.



An example of neutral background colours, strong paving colour and some highlight colour for vibrancy.



Above are examples of suitable types of non dominant background colours for walls, roofs and external structure. Pure whites are not recommended background colours.



Above are examples of suitably strong and vibrant highlight colours.

# Precinct Plan Report

## 17.0 SERVICES

- 17.1 All plant and equipment associated with mechanical services will be housed either within the building or its roof space or well screened at ground level.
- 17.2 All waste storage and delivered goods will be contained within the associated buildings.
- 17.3 Doors providing access to internal waste/storage or loading dock areas will be the minimum width and height possible to serve the required loading/unloading function and be constructed of aesthetically pleasing materials.
- 17.4 Awnings or canopies associated with loading doors will be designed to complement the scale, material and style of other awnings used elsewhere on the building.

## 18.0 TRAFFIC NOISE

- 18.1 The development will be subjected to noise from traffic on Princess Royal Drive and the railway therefore the design of any accommodation buildings should include devices or systems to ameliorate problems arising from airborne sound. The use of double glazing is recommended.
- 18.2 All accommodation buildings are to be detailed and/or designed so that internal noise levels in bedroom areas will be in the range of 30 to 35dB.

Rev

## 19.0 SAFETY AND SECURITY

- 19.1 Provide appropriate lighting to all pedestrian paths, parking areas and building entries.
- 19.2 Buildings are to be designed to overlook public spaces.
- 19.3 Building entries are to be clearly visible from public spaces.

Rev: Use of double glazing recommended.

# Precinct Plan Report

19.4 Materials vulnerable to graffiti and vandalism are to be avoided and robust materials which are aesthetically pleasing are to be used in all public places.

19.5 In mixed-use developments a diversity of complementary uses should be provided to encourage a public presence at different times of the day and night.

19.6 Landscaping, walls and fencing will be designed to maintain clear visibility to and from doors, windows, and pedestrian ways.

## **20.0 EXTERNAL LIGHTING**

20.1 Exterior lighting will minimise skyward light spill.

20.2 All external lighting will be robust and themed to complement the development as a whole.

## **21.0 ENVIRONMENT**

21.1 All stormwater to be contained on site or connected to drainage points where supplied.

21.2 To minimise the nutrient run-off to Princess Royal Harbour, all private and public open spaces should be designed and maintained to minimise fertilising and excess watering.

## **22.0 MARINA AND WATER'S EDGE**

22.1 Breakwaters will be designed to provide appropriate protection to the boat facilities and land development from the action of the ocean in Princess Royal Harbour. The breakwaters will be constructed in the conventional method used throughout Western Australia using quarried rock with large stones as armour on both the inside and outside faces. There will be a maintenance road way on the top of each breakwater. Maintenance campaigns are expected to take only a month or

# Precinct Plan Report

so to complete and be required about every 5 years or so. The actual timing will depend on the storm activity experienced.

- 22.2 The development includes a number of simple rock revetments and a marina edge walling used to stabilise the land / water interface. These structures will be constructed from quarried rock and reconstituted limestone blocks. Over the lifetime of these structures, there will be the need for some minor maintenance. This is expected to be needed about every 5 to 10 years and will require access for the equipment, materials and workmen.
- 22.3 The marina will include a number of floating pens for the mooring of recreational and fishing industry vessels. The pens will be held in place by a number of guide piles driven into the seabed and floating pens will rise and fall with the tide. The pen operator will need to carry out minor routine maintenance on the pens on a monthly basis and probably complete significant maintenance campaigns every 10 years. The access for these maintenance activities will be from the commercial boating areas.

- 22.4 A portion of the existing Town Jetty is to be retained within the proposed new breakwater. A building is proposed to be developed in close proximity to the jetty. An important part of the design of this building will be the inclusion of a continuous timber walkway facing the marina. This walkway is to be approximately 5m wide and tie in with the existing jetty.
- 22.5 Pedestrian access to the marina pens is to be controlled via lockable gates. These gates will be designed to complement the general aesthetics of the development.
- 22.6 Any gates and fencing to be constructed in the Town Jetty Precinct will be designed to complement the general aesthetics of the development.

# Precinct Plan Report

## 23.0 SITE FACILITIES

23.1 Garbage collection methods will comply with the general requirements of the City of Albany and will be efficient, convenient and allow for collection of recyclable material if possible.

23.2 Refuse and bin storage areas will be provided at the Boat Trailer Hard Stand area and the Fishing Industry Hardstand area. These facilities will be screened from public view using materials and forms that complement the development.

## 24.0 SHORT-STAY RESIDENTIAL

24.1 No permanent residential developments are permitted in the Albany Waterfront.

### NOTE:

Where this document refers to 'short-stay' this shall mean that the maximum length of stay in any twelve month period is three months in accordance with WAPC Planning Bulletin 83.

Rev 17/10/11

24.2 Short-stay serviced apartment buildings shall conform to the standards and requirements applicable to the R-IC provisions of the Residential Planning Codes.

24.3 Serviced apartments must not front the Promenade at ground level and are not generally encouraged at ground level unless it can be demonstrated that satisfactory private open space provisions can be achieved.

## 25.0 SITE PLANNING

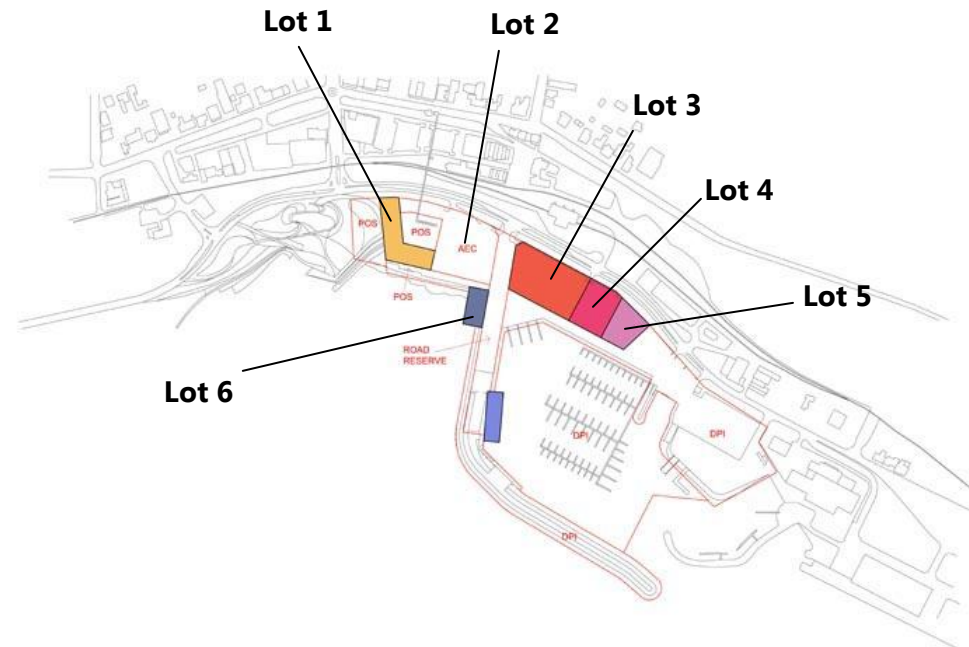
25.1 The Structure and Precinct Plans provide approximate footprints for buildings on all sites. These footprints, while being only notional, are an indication of the general plan form desired for each building in the development.

25.2 Lot 2, containing the AEC, has the potential to vary its footprint considerably depending on the final design of this facility however, the siting and design principles outlined elsewhere are to be adhered to.

# Precinct Plan Report

25.3 Lots 1, 3, 4 and 5 show combinations of 'L' shaped plan forms which are intended to enable buildings to address the Promenade on the south and at the same time provide wind protection for outdoor spaces on the north side.

25.4 Lots 6 shows a rectangular plan forms intended to maximise frontages to the marina and Toll Place.



# Precincts

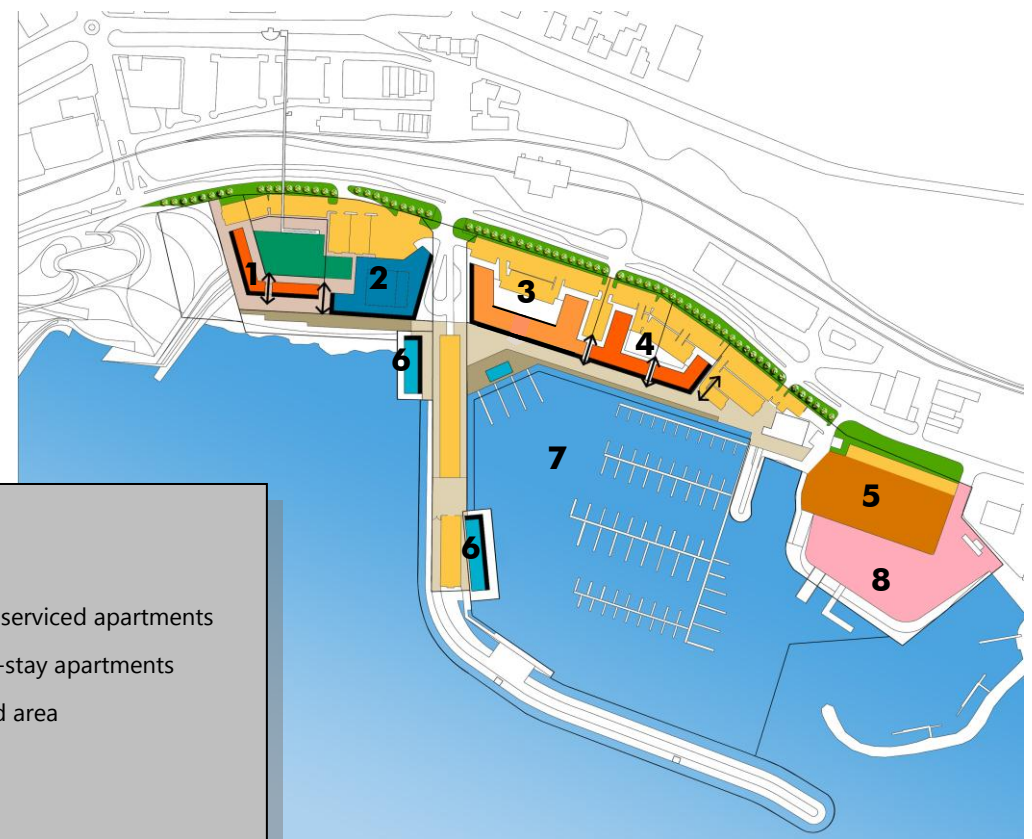
The Albany Waterfront has been broken into five precincts which are characterised primarily by different land uses. Planning provisions for each precinct are described in the following section.

## Precincts



- 1. Entertainment Precinct
- 2. Accommodation Precinct
- 3. Commercial Precinct
- 4. Harbour Precinct
- 5. Town Jetty Precinct

## Development Uses

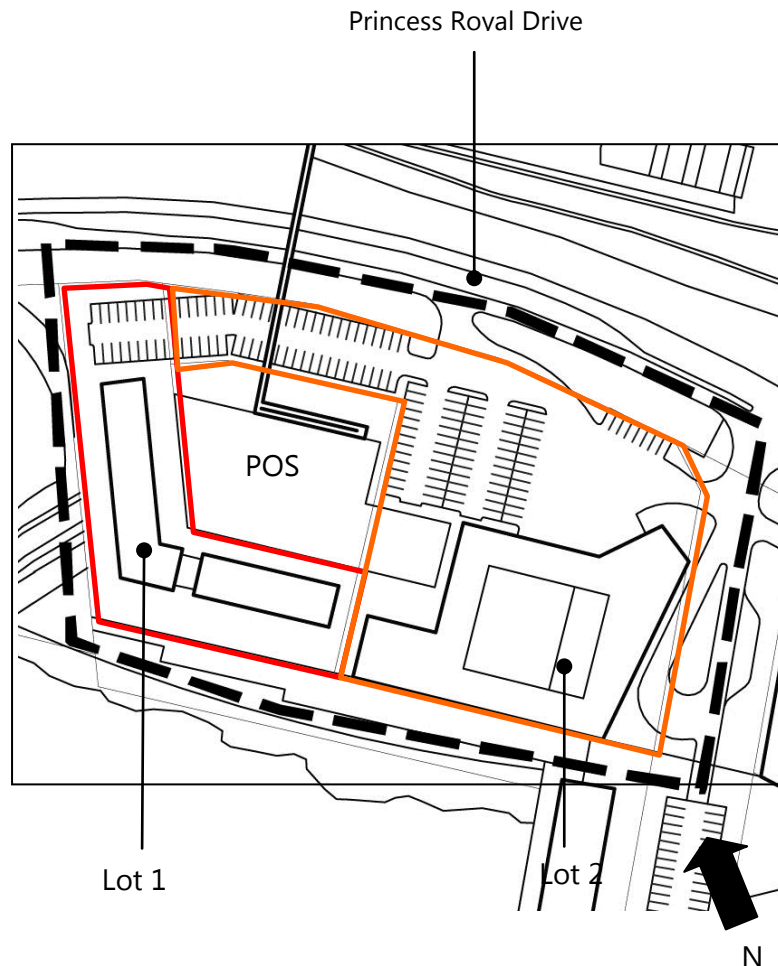


- 1. Mixed use
- 2. AEC
- 3. Hotel and short-stay serviced apartments
- 4. Mixed-use and short-stay apartments
- 5. Boat trailer hardstand area
- 6. Retail
- 7. Marina
- 8. Fishing area hardstand

Rev: Inclusion of short-stay apartments in item 4

# Precinct Plan Report

## Entertainment Precinct



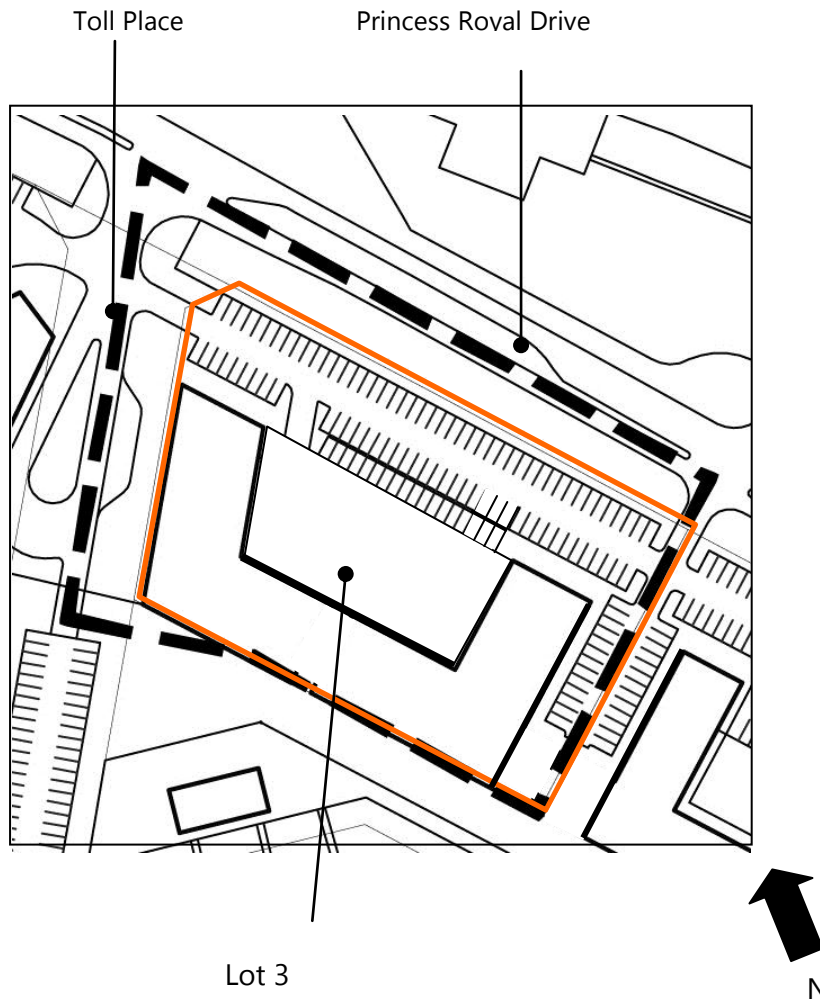
Note: "Potential to use roof volume" means using the available roof volume to provide the spaces below with mezzanine floors or loft spaces.

<b>Building height</b>	On Lot 1 to be 2 storeys with potential to use roof volume. Albany Entertainment Centre will have variable heights determined according to the function of particular zones within the centre. The fly tower is the tallest component at 37m.
<b>Plot ratio</b>	Plot ratio shall be a maximum of 0.5 for all lots in the precinct
<b>Parking</b>	15 car bays shall be provided on Lot 1 130 car bays shall be provided on Lot 2
<b>Setbacks</b>	<p>Lot 1:</p> <ul style="list-style-type: none"> <li>25m minimum from Princess Royal Drive.</li> <li>5m minimum from western boundary.</li> <li>5m minimum from the Promenade.</li> <li>4m minimum from POS boundaries.</li> <li>4m minimum from eastern boundary.</li> </ul> <p>Lot 2:</p> <ul style="list-style-type: none"> <li>25m minimum from Princess Royal Drive.</li> <li>4m minimum from POS.</li> <li>4m minimum from western boundary.</li> <li>Nil setbacks from all other boundaries.</li> </ul>



# Precinct Plan Report

## Accommodation Precinct



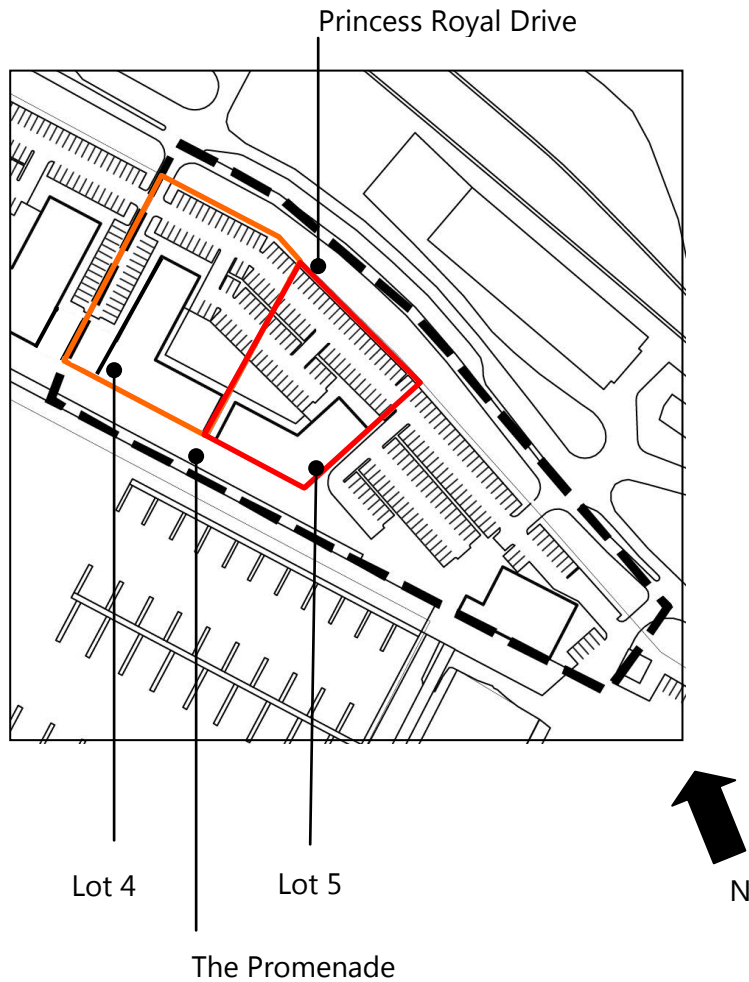
<b>Building height</b>	The hotel building facing Toll Place to be 5 storeys. The adjacent serviced apartment building to be 6 storeys. A building element used to signify the entry points may project beyond the roof to a maximum of 3 metres.
<b>Plot ratio</b>	Plot ratio shall be a maximum of 2.5:1 for all lots in the precinct.
<b>Parking</b>	A minimum of 222 car bays to be provided on Lot 3. This comprises approximately 83 open bays and a minimum of 139 undercover bays.
<b>Setbacks</b>	25m minimum from Princess Royal Road. 12m minimum from eastern boundary to 6 storey wall of serviced apartment building. Nil setbacks from all other boundaries.

Rev

- Rev:
- Increase in plot ratio and car parking requirements.
  - Deleted reference to use of roof spaces.
  - Increase in permitted building height.
  - Plan updated to show undercover parking area on northern side.
  - Deleted required break in building edge facing promenade.
  - Deleted single storey portion on south east corner.

# Precinct Plan Report

## Commercial Precinct



<b>Building height</b>	3 storeys, provided the third floor is used exclusively for short-stay accommodation, otherwise height is limited to 2 storeys.	Rev
<b>Plot ratio</b>	Plot ratio shall be a maximum of 0.5 for all lots in the precinct.	
<b>Parking</b>	65 car bays to be provided on Lot 4. 65 car bays to be provided on Lot 5. 105 car bays to eastern side of Lot 5.	
<b>Setbacks</b>	<p>Lot 4:</p> <p>25m minimum from Princess Royal Road. 12m minimum from western boundary wall. Nil setback from the Promenade.</p> <p>Lot 5:</p> <p>25m minimum from Princess Royal Road. 2.5m minimum from western boundary. Nil setback from all other boundaries.</p>	

Rev: Building height increased to 3 storeys provided third floor is exclusively used for short-stay accommodation.

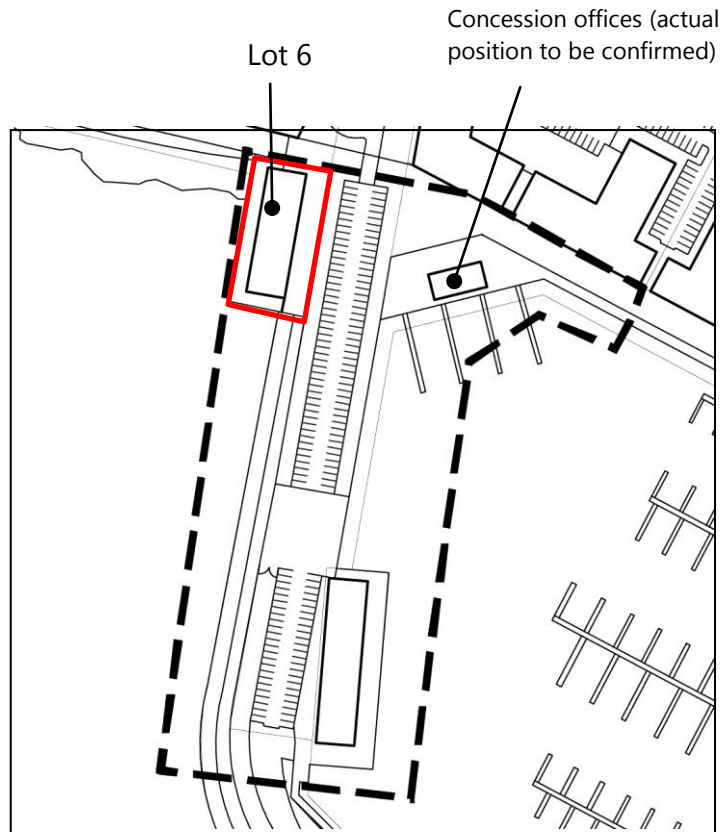
Deleted reference to use of roof spaces.

Rev: Delete reference to setback from single storey

wall on western boundary of Lot 4

# Precinct Plan Report

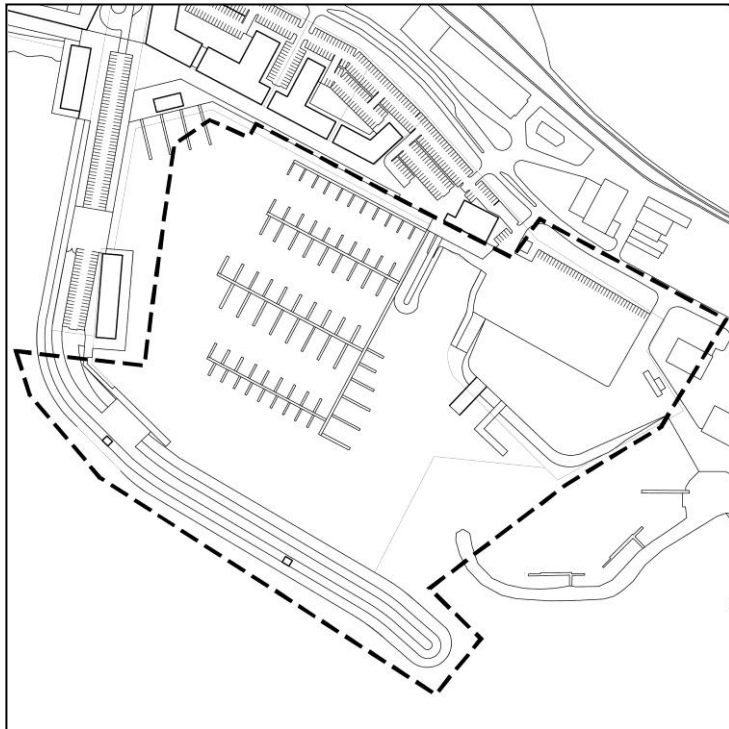
## Town Jetty Precinct



<b>Building height</b>	Lot 6 is to be single storey. Concession offices associated with charter boats to be single storey.
<b>Plot ratio</b>	Plot ratio to lot 6 shall be a maximum of 0.35.
<b>Parking</b>	145 cars to be provided along the old Town Jetty.
<b>Setbacks</b>	No required setbacks.

# Precinct Plan Report

## Harbour Precinct



<b>Building height</b>	Any buildings in this precinct to be single storey.
<b>Plot ratio</b>	Not applicable.
<b>Parking</b>	45 permanent car bays to boat trailer hard stand area.
<b>Setbacks</b>	Minimum set backs for any new buildings in the fishing industry hardstand area will be 2.5m from eastern boundaries. If buildings are used to store hazardous materials then the setbacks will be determined according to safety requirements.



# Precinct Plan Report

## Land Use Tables

### Entertainment Precinct

Land Table	Use	Use Class	Definition	Permissibility
Refer to Clause 3.4 of TPS1A for the meaning of the symbols		Theatre/Entertainment Centre (on Lot 2 only)	being premises used by the public to view a theatrical or similar productions or for the display, or display and sale, of materials of an artistic, cultural or historic nature	P
		Market	See TPS1A	AA
		Shop	See TPS1A	P
		Restaurant	See TPS1A	P
		Office	See TPS1A	AA
		Other minor or incidental uses within the precinct may be permitted at the discretion of Council		n/a

# Precinct Plan Report

## Accommodation Precinct

Land Use Table	Use Class	Definition	Permissibility
Refer to Clause 3.4 of TPS1A for the meaning of the symbols	Hotel	See TPS1A	P
	Motel	See TPS1A	P
	Residential Building (short-stay accommodation only)	See TPS1A	AA
	Reception Centre	being premises used for functions on formal or ceremonial occasions but not for un-hosted use for general entertainment purposes	IP
	Shop	See TPS1A	IP
	Restaurant	See TPS1A	IP
	Office	See TPS1A	IP
	Other minor or incidental uses within the precinct may be permitted at the discretion of Council	n/a	SA

# Precinct Plan Report

## Commercial Precinct

Land Use Table	Use Class	Definition	Permissibility
Refer to Clause 3.4 of TPS1A for the meaning of the symbols	Shop	See TPS1A	P
	Office	See TPS1A	AA
	Restaurant	See TPS1A	AA
	Museum	See TPS1A	AA
	Residential Building (short-stay accommodation only)	See TPS1A	AA
	Other minor or incidental uses within the precinct may be permitted at the discretion of Council	n/a	SA

Rev: Addition of residential use

## Town Jetty Precinct

Land Use Table	Use Class	Definition	Permissibility
Refer to Clause 3.4 of TPS1A for the meaning of the symbols	Restaurant	See TPS1A	P
	Shop	See TPS1A	P
	Office	See TPS1A	IP
	Other minor or incidental uses within the precinct may be permitted at the discretion of Council	n/a	SA

# Precinct Plan Report

## Harbour Precinct

Land Use Table	Use Class	Definition	Permissibility
Refer to Clause 3.4 of TPS1A for the meaning of the symbols	Marina	See TPS1A	P
	Marine Filling Station	See TPS1A	P
	Marine Services Facilities	See TPS1A	P
	Other minor or incidental uses within the precinct may be permitted at the discretion of Council	n/a	SA

Rev

Rev: Marine Services Facilities added as a permissible use



# Precinct Plan Report

## Implementation

### Structure Plan

The Structure Plan upon which this Precinct Plan is based was adopted by Council on the 16<sup>th</sup> May 2006.

The Precinct Plan complies with and is substantially in accordance with the Structure Plan.

### Commitments

- 1) The landowner lodged applications for subdivision and development and commenced development of the site in late September 2006. Rev
- 2) The preliminary subdivision plan showing all boundaries and dimensions of all lots to be created is submitted with this Precinct Plan.

- 3) Development is anticipated to be staged in accordance with a program to be lodged with future development applications.

- 4) A detailed Landscape Master Plan has been completed. Rev

- 5) Princess Royal Drive has been modified during the development process in accordance with the Albany Waterfront Traffic Assessment report prepared by Riley Consulting and included in the Structure Plan. Rev

- 6) Details describing type and colour of paving materials proposed for construction of roads, paths and public open spaces have been submitted and approved. Rev

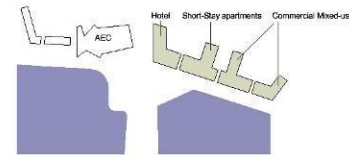
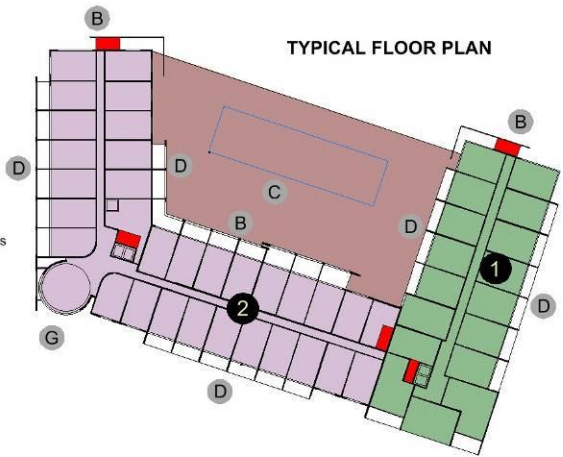
Rev: Updated to reflect status of commitments

# Precinct Plan Report

## **Addendum 1**

**Supporting information associated with proposed amendments to structure and precinct plans initiated by Landcorp in 2011.**

- Hotel Lobby
- Covered parking
- Egress points
- Meeting rooms  
Approximately 400m<sup>2</sup> shown
- Tavern, bar, restaurant  
Approximately 600m<sup>2</sup> shown
- Short-stay apartments above commercial mixed-use buildings
- Back of house
- Short-stay apartments on five floors  
(60m<sup>2</sup> average apartment size shown)
- Hotel rooms on four floors  
55m<sup>2</sup> average room size shown
- Covered parking
- Egress points
- Tavern, bar, restaurant  
Approximately 600m<sup>2</sup> shown
- Short-stay apartments above commercial mixed-use buildings



**DEVELOPMENT GUIDELINES**

- A** All building frontages to maximize glazing and allow for visual permeability and where appropriate should flow out to encourage public activity at promenade level
- B** All building ends facing Stirling Terrace are to maximize glazing and use of balconies in order to articulate and 'enliven' these building faces
- C** Maximize vegetation on deck over parking area to soften view from Stirling Terrace and to reduce heat sink effects. Vegetation is to be in the form of ground cover rather than scattered trees in planters in order to cover as much of the paved surface as possible
- E** All delivery points to service back of house facilities must not be within sight of Stirling Terrace
- D** All apartments must include balconies in order to modulate building facades and ensure buildings reflect human scale
- F** Consideration should be given to using 'green roofs' on these buildings as they could be viewed from some portions of Stirling Terrace
- G** Entrances to these buildings must be well defined and in keeping with the very public nature of the Promenade

**SUMMARY**

Buildings within the Accommodation and Commercial Precincts of the Albany Waterfront Development are proposed to be increased to enable a higher development yield to occur. This is in response to feedback from potential operators/owners who advise the current capacity of these buildings is too low for viable operation.

The following increases to the Hotel, Short-stay Apartments and the Commercial Mixed-use buildings are proposed:

- Increase Short-stay Apartments from four to six floors
- Increase Commercial Mixed-use buildings from two to three floors

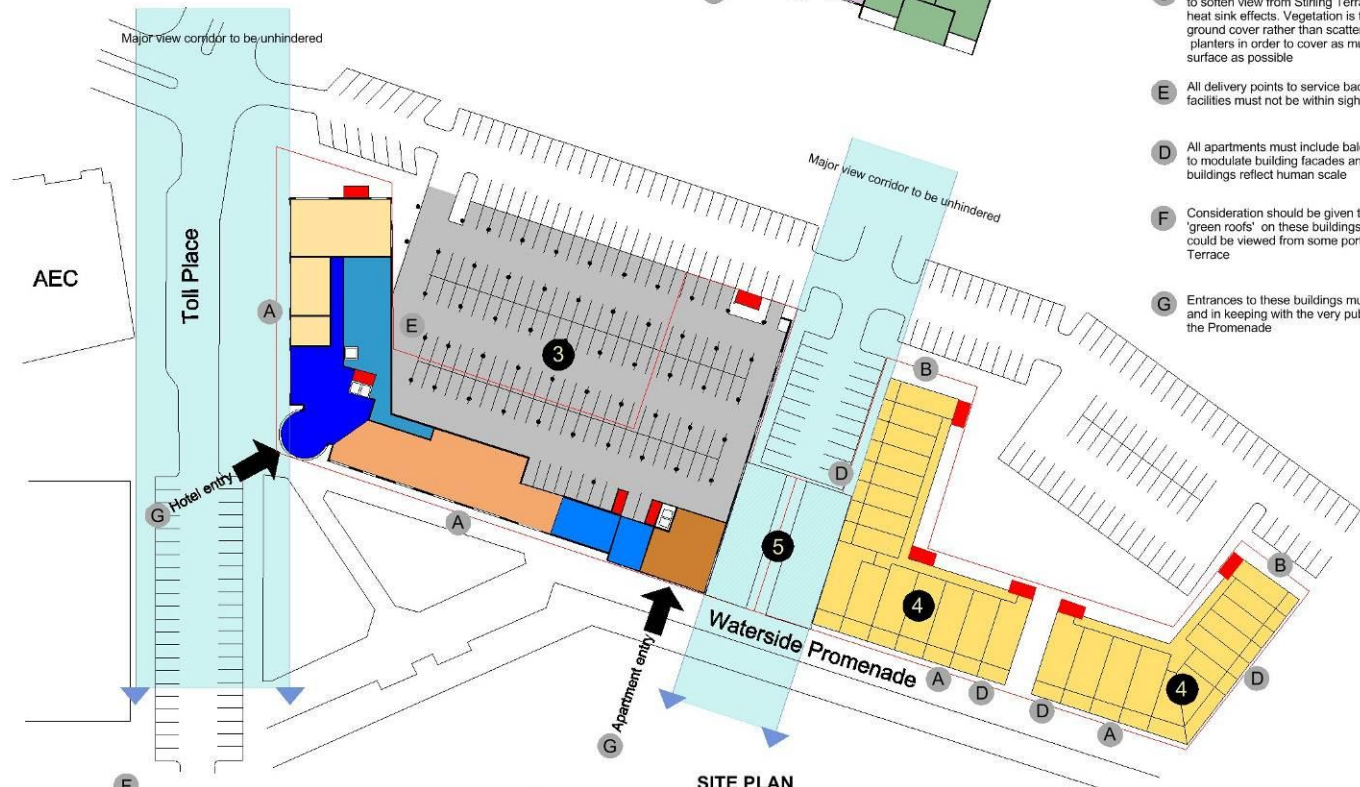
These height increases result in the following:

120 to 145 hotel rooms  
134 to 144 short-stay apartments  
A GFA increase from 16,600m<sup>2</sup> to 24,050m<sup>2</sup>

	Current	Proposed
Hotel building	7,500m <sup>2</sup> GFA	10,500m <sup>2</sup> GFA
Short-stay apartment building	4,600m <sup>2</sup> GFA	6,800m <sup>2</sup> GFA
Commercial Mixed-use building	4,000m <sup>2</sup> GFA	6,750m <sup>2</sup> GFA
Car parking	700 bays	792 bays

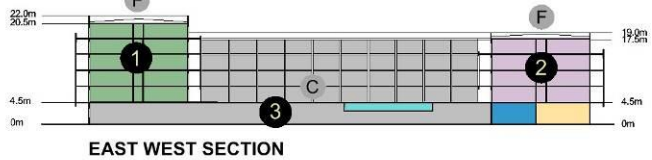
**PROPOSED CHANGES TO STRUCTURE PLAN**

- 1** The short-stay apartments in the accommodation precinct increased from four to six floors. This increase will take the GFA from 4,600m<sup>2</sup> as noted in the Structure Plan to 6,800m<sup>2</sup>. The configuration shown here is capable of providing 80 to 90 short-stay apartments depending on mix of apartment sizes.
- 2** The hotel portion of the accommodation precinct is shown here as a continuous building adjoining the short-stay apartments. The footprint shown produces a hotel with a GFA of 10,500m<sup>2</sup> which is 3,000m<sup>2</sup> larger than the estimated hotel area noted in the Structure Plan. The hotel in this configuration is capable of providing 120 to 145 rooms depending on the mix of room sizes.
- 3** Covered parking for 130 cars can be provided on this site. There are presently 38 car bays in this area which have reciprocal use arrangements and must maintain this status. The additional 92 bays will be for the exclusive use of hotel and short-stay apartment guests.
- 4** These two buildings in the commercial precinct are increased from two to three floors provided the top floor is used as short-stay accommodation. These buildings are designated mixed-use in the Structure Plan. Their approximate GFA is currently 4,500m<sup>2</sup> which will increase to 6,750m<sup>2</sup> with an additional floor. As a mixed-use development the current building would require 150 car bays. If the development became mixed-use at ground level and short-stay apartments above the car bays required would reduce to approximately 130 bays, depending on the number of apartments (54 apartments are shown as only a suggestion of potential numbers).
- Mixed-use at ground level may be concentrated to the waterside promenade and the rear portions can be ancillary to the short-stay apartments. No apartments are to be at ground level
- 5** The area between these 2 buildings is to be used for public purposes and cannot be built upon



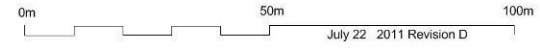
**SITE PLAN**

NOTE: It is not intended that the plans shown here are to be replicated by a future owner/operator. The aim is to show what is possible and the extent of changes being proposed to the Structure Plan. Future development of the site will be required to follow these guidelines and maintain, approximately, the building footprints shown.

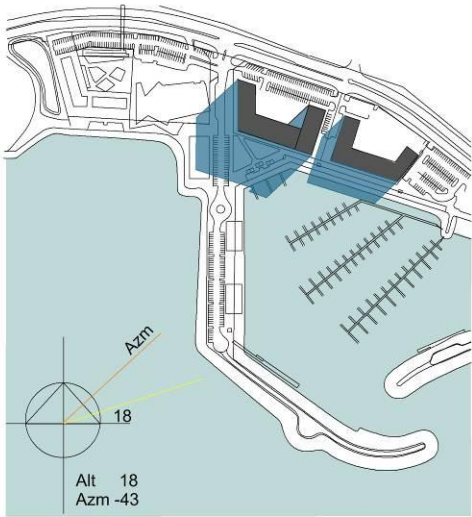


**EAST WEST SECTION**

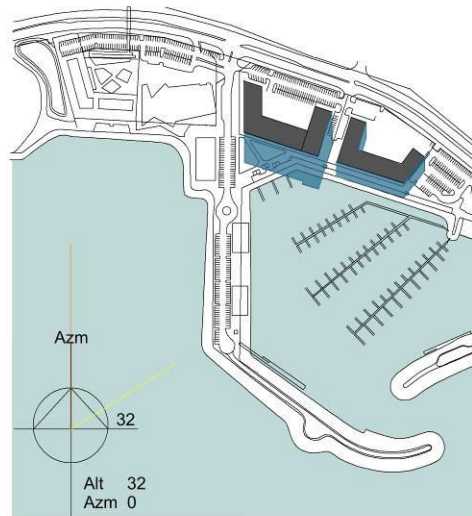
**PROPOSED CHANGES TO THE ALBANY WATERFRONT STRUCTURE PLAN**



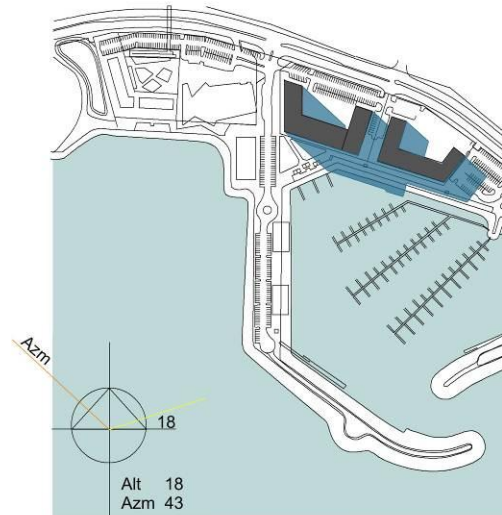
9.00am June 21



Noon June 21



3pm June 21

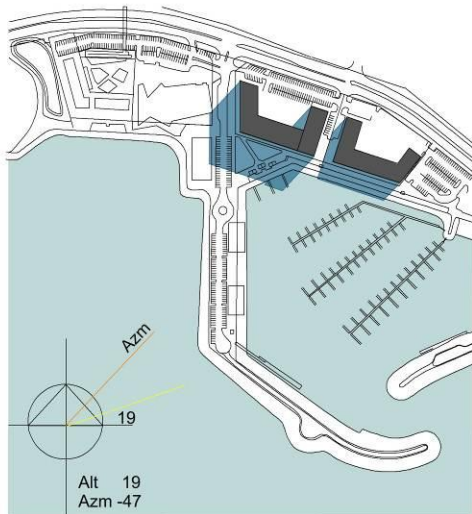


This shadow study produced at the request of the City of Albany. The diagrams show that shadows cast during summer and winter have a negligible impact on the shoreline.

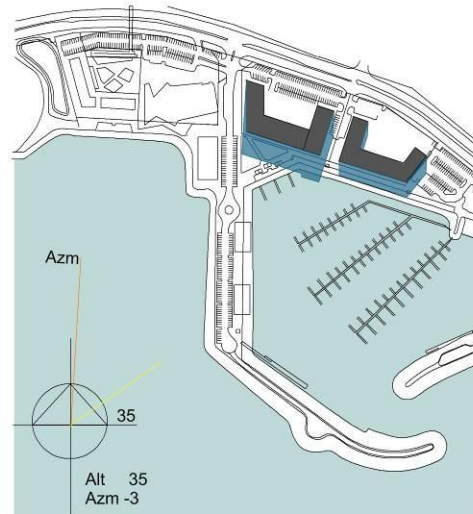
At 35deg South the sun's path differs little between summer and winter.

### Albany Waterfront 35 deg South Mid Winter

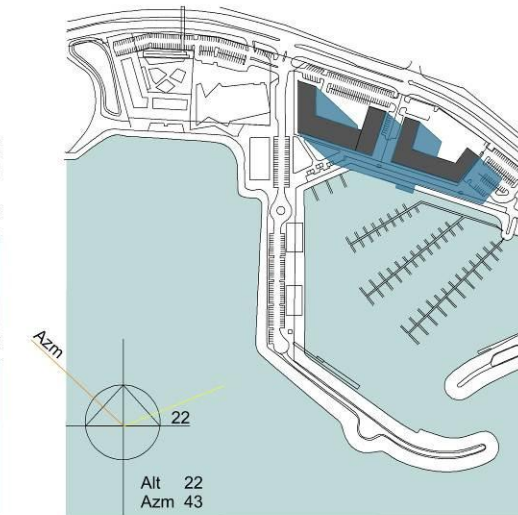
9.00am Jan 21



Noon Jan 21



3pm Jan 21



SHAPING OUR HABITAT



urbanixdesign.com.au

### Albany Waterfront 35 deg South Mid Summer

Study date 18 July 2011

# Precinct Plan Report

## VIEW FROM PEDESTRIAN BRIDGE



Above - Possible development scale currently achievable



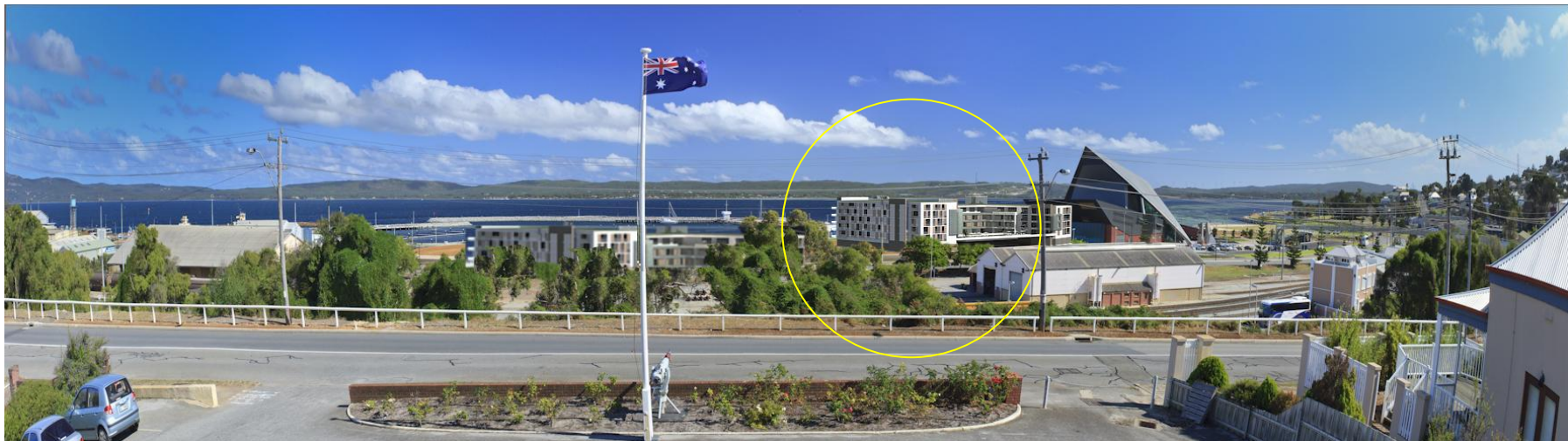
Above - Possible development scale under proposed amendments

The following series of images show the mass and scale of buildings achievable under the current approved development framework compared with the mass and scale achievable under the proposed changes. Please note that the purpose of these images is not to convey building design. The actual appearance of these buildings will ultimately depend on the individual developers who will be required to develop buildings in accordance with the Precinct Plan design guidelines.

# Precinct Plan Report

**VIEW FROM STIRLING TERRACE**

Below - Possible development scale currently achievable



Above - Possible development scale under proposed amendments

# Precinct Plan Report

**VIEW FROM STIRLING TERRACE**

Below - Possible development scale currently achievable



Above - Possible development scale under proposed amendments

# Precinct Plan Report

## VIEW ALONG THE SPENCER STREET VIEW CORRIDOR

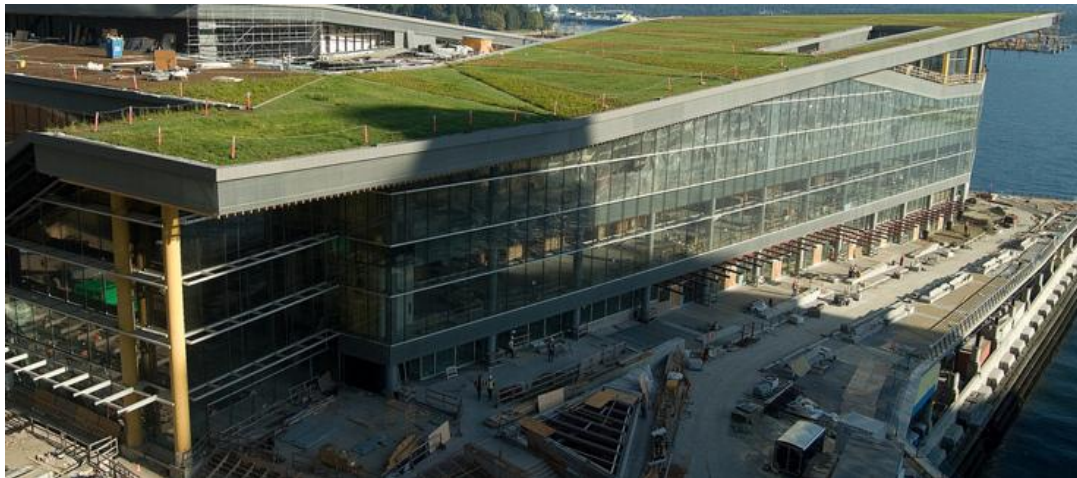
Below - Possible development scale currently achievable



Above - Possible development scale under proposed amendments



## Precinct Plan Report



The use of green roofs is encouraged if suited to future proposed developments. These images show how green roofs provide softening to large roof areas.